



The CITY OF DAYTONA BEACH

REDEVELOPMENT DIVISION

POST OFFICE BOX 2451

DAYTONA BEACH, FLORIDA 32115-2451

PHONE (386) 671-8180

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AGENDA

DOWNTOWN/BALLOUGH ROAD REDEVELOPMENT AREA BOARD

Tuesday, June 2, 2015 12:00 P.M.

City Hall Commission Chambers

NOTICE: If any person decides to appeal any decision of the Board at this meeting, they will need a record of the proceedings. Interested persons may wish to ensure that a verbatim record of the proceedings before the Board is made, including any testimony or evidence presented to the Board. The City does not prepare or provide a verbatim record of Board proceedings.

1. **Call to Order**
2. **Roll Call**
3. **Approval of the minutes: May 5, 2015**
4. **DEV 2015-024, SITE PLAN, American Born Salvage, continued**
Request by Colleen Miles, Land Development Resource Group, LLC, on behalf of American Born Salvage for site plan approval in the M-1 zoning district. The site is located at 142 South Segrave Street. (Reference: 1993 Land Development Code, as revised, Article 4§8.2)
5. **Redevelopment Project Updates**
6. **Public Comments**
7. **Board Comments**
8. **Adjournment**

DOWNTOWN REDEVELOPMENT BOARD
MINUTES
Tuesday, May 5, 2015

A meeting of the Downtown Redevelopment Board was held Tuesday, May 5, 2015, at 12:00 p.m. in the Commission Chambers, 301 S. Ridgewood Avenue, Daytona Beach, Florida. The following people were present:

Board Members Present

Mr. Harold Goodemote, Chair
Mr. Robert Abraham
Mr. Buddy Budiansky
Ms. Quanita May
Dr. Kent Sharples
Mr. Michael Shewmaker
Mr. Jack White

Board Members Absent

Ms. Cathy Washington

Staff Members Present

Mr. Reed Berger, Redevelopment Director
Mr. Jason Jeffries, Redevelopment Project Manager
Ms. Carrie Avallone, Assistant City Attorney
Ms. Becky Groom, Board Secretary

1. Call to Order

Mr. Goodemote called the meeting to order at 12:10 p.m.

2. Roll Call

Ms. Groom called the roll and noted members present as stated above. Mr. Goodemote welcomed Mr. Shewmaker, Mr. Budiansky, and Ms. May to the board.

3. Approval of Minutes – February 3, 2015

Board Action:

Dr. Sharples made a motion to approve the minutes of the Regular Meeting of February 3, 2015. Mr. Abraham seconded the motion and it was approved unanimously (7-0).

4. **Election of Officers**

Election of Chair

Board Action:

A motion was made by Mr. Abraham, seconded by Mr. White, to appoint Mr. Goodemote as Chairman. The motion carried unanimously (7-0).

Election of Vice Chair

A motion was made by Dr. Sharples, seconded by Mr. Abraham, to appoint Mr. White as Vice Chair. The motion carried unanimously (7-0).

5. **American Born Salvage**

Mr. Jeffries presented the staff report which is included as part of the packet. Mr. Jeffries stated this project was submitted prior to the adoption of the new Land Development Code and stated this project was reviewed under the 1993 Land Development Code. Mr. Jeffries stated staff recommends approval of the Conditional Use request. Mr. Jeffries stated the Site Plan requires approval of the variance regarding reduction of the number of parking spaces by the Board of Adjustment. Mr. Jeffries stated this item was scheduled for the May 21, 2015, Board of Adjustment meeting.

Applicant's Presentation:

Colleen Miles, Land Development Resource Group, spoke representing the applicant. Ms. Miles stated a truck will enter the building to unload and then back into the right of way, which was how the vehicles historically have exited the site. Ms. Miles stated all fluids will be removed from the vehicles prior to being loaded onto the transport vehicle to be taken to this site. Ms. Miles stated a new floor will be installed that was a solid slab of concrete that has been treated and will meet FEMA standards.

Mr. White expressed concern about the design of the building. He stated things could be done to treat the building in a more historical way as opposed to appearing as a garage.

Ms. Miles stated she welcomed suggestions on improvements that could be made to enhance the aesthetics of the building.

Ms. Avallone stated the Conditional Use could be voted on today; and she stated the board could determine that they would like to see the site plan prior to taking a vote.

Mr. White stated he did not feel he had enough information to decide on the design at this time.

Ms. Avallone asked Ms. Miles if she had an objection to delaying the site plan review until the June meeting.

Ms. Miles stated that would be fine.

Mr. Abraham asked if there will be noise issues that may disturb the neighborhood.

Ms. Miles stated the facility will operate within the hours outlined in the Code.

Mr. Abraham asked if the neighbors are aware of this request.

Ms. Miles said yes.

Public Comments:

John Nicholson, 413 N. Grandview, stated he would like to see the old Cracker style of the building retained.

a. **DEV 2015-024, Site Plan**

Board Action:

A motion was made by Dr. Sharples, seconded by Mr. White, to continue DEV2015-024, Site Plan, to the June Downtown Redevelopment Board Meeting. The motion carried unanimously (7-0).

A motion was made by Dr. Sharples, seconded by Mr. White, to recommend approval to the Board of Adjustment for the variances that are requested and outlined in the staff report. The motion carried unanimously (7-0).

b. **DEV 2015-025, Conditional Use**

A motion was made by Dr. Sharples, seconded by Mr. White, to approve DEV2015-025, Conditional Use, as presented in the staff report. The motion carried unanimously (7-0).

6. Redevelopment Project Updates

Mr. Jeffries distributed a report that outlined the budget for the Downtown and Ballough Road Redevelopment areas.

Mr. Budiansky asked where he could see line items for the budget and asked about the grant programs that are listed on Page 2.

Mr. Jeffries stated he was presenting a brief overview of the Downtown and Ballough Road Redevelopment Area budgets. He stated the grant figure includes the residential and commercial grants in the Downtown Redevelopment area.

Ms. May asked for a brief description of what was included in the budget items.

Dr. Sharples asked about the debt service amount that was listed for \$185,000.

Mr. Jeffries stated the debt service was for the Magnolia streetscape project and the parking lot purchase for Beach Street.

Mr. Berger stated the detailed CRA budgets are on the City's web site for the Board's review.

Mr. Jeffries stated the contract with Mr. Gibbs was still open and he has talked with him about re-activating that contract for recruitment. Mr. Jeffries stated there are funds allocated in the budget for the Parker Mynchenberg study for the Beach Street streetscape.

Mr. Goodemote asked what Mr. Gibbs has brought or recruited to downtown Daytona.

Mr. Berger stated the idea was to finish up the recruitment and he feels we do not have complete performance from Mr. Gibbs and he was hoping for some success now that the economy has improved.

Mr. Jeffries stated retailers are starting to look at the Daytona Beach area but currently look west of Clyde Morris due to the demographics and accessibility of the highway.

Mr. Budiansky stated he believes there was a contract for purchase on the former car lot properties.

Mr. Berger stated the image that some retailers see because of the homeless situation discourages those retailers. Mr. Berger stated he was excited about the progress that was being made regarding the homeless situation.

Mr. Jeffries stated expenses listed in the report at \$156,000 are for two long-term agreements, i.e., Sunset Harbor and Caribbean Jack's. Mr. Jeffries stated the facility at Sunset Harbor was still open so they are in compliance with their development agreement.

Mr. Goodemote asked about the business located in the building north of Kingston Ave.

Mr. Jeffries stated he would have Code Enforcement look into Mr. Goodemote's concerns.

Mr. Berger stated Beth Lemke has been working on FIND agreements for the Ballough Road area and was working on a grant for the north esplanade of Riverfront Park. Mr. Berger stated this project would bring boaters from the river into the downtown as was outlined in the Riverfront Master Plan. Mr. Berger stated Ms. Lemke was also working on an RFP for the bridge at the Root Canal.

Mr. Goodemote asked about the ISB streetscape.

Mr. Berger stated it was in progress but discussion continues on the traffic circles.

Mr. Goodemote asked if there was any progress on the former auto lots.

Mr. Berger stated interest has been expressed in the site but there was nothing under contract that he was aware of. Mr. Berger stated there was a priority to start working on a parking garage in the downtown and staff was trying to get shared parking in the middle block.

Mr. Budiansky asked if 15 minute parking limits could be placed at certain locations along Beach Street.

Mr. Jeffries stated there would be an issue as to where time limit signs would be placed and in front of which businesses. He stated enforcement would be an issue.

Mr. Goodemote asked if there was any progress with the WROD radio station and restaurant request that the Board approved.

Mr. Jeffries stated the project was under construction.

7. Public Comments

John Nicholson, 413 N. Grandview, stated submerged lands have not been addressed by the CRA and Planning Board. Mr. Nicholson stated there are five different street teams cleaning City streets. He expressed concern about the appearance of City Island.

Pollyann Brazina, 3412 S. Atlantic Avenue, stated she was attending the meeting as a representative of the leadership academy for the Daytona Beach Board of Realtors.

8. Board Comments

There were no Board comments.

9. Adjournment

There being no further business to come before the Board, the meeting was adjourned at 1:25 p.m.

Harold Goodemote, Chair

Becky Groom
Recording Secretary



THE CITY OF DAYTONA BEACH

REDEVELOPMENT DIVISION

POST OFFICE BOX 2451
DAYTONA BEACH, FLORIDA 32115-2451
PHONE (386) 671-8180
Fax (386) 671-8187

MEMORANDUM

DATE: May 28, 2015

TO: Downtown Redevelopment Board Members

FROM: Jason Jeffries, Project Manager

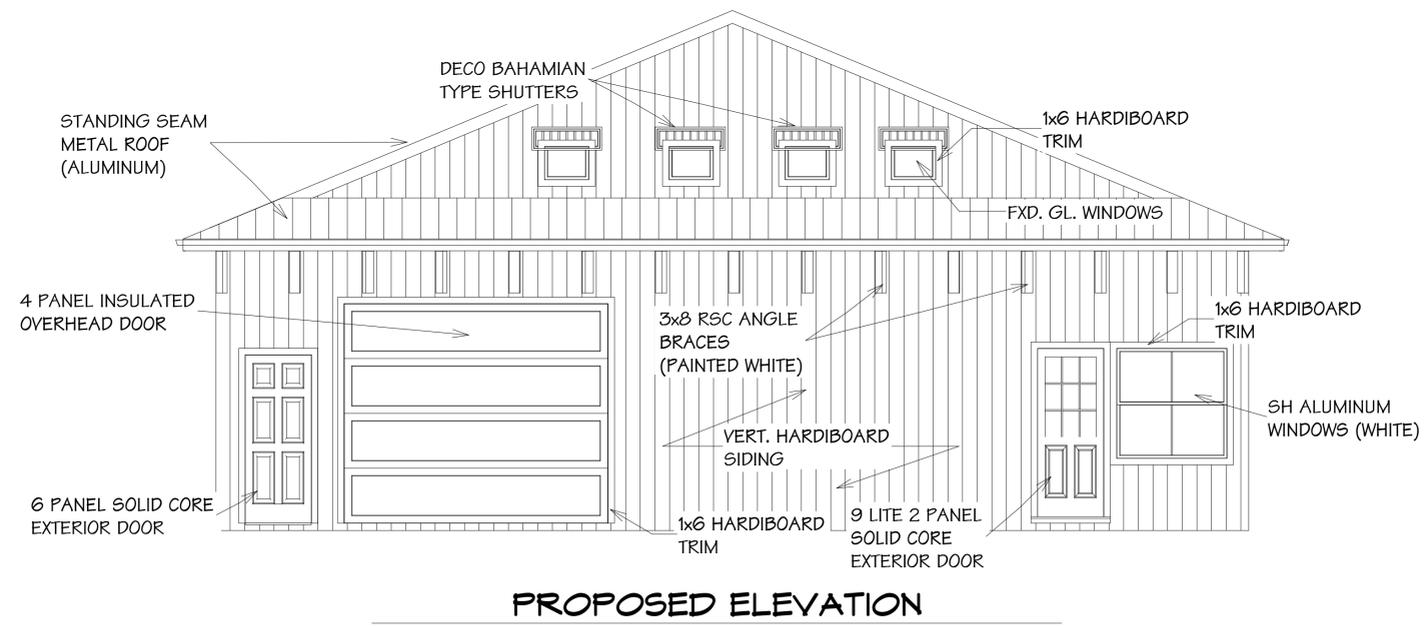
SUBJECT: DEV 2015-024: American Born Salvage

Enclosed is a revised front architectural elevation for your review. Based on comments from the last meeting, the applicant made the following changes to the proposed façade:

1. Retained the following architectural features of the structure:
 - a. Shutters above the upper elevation vents / windows
 - b. Roof braces under the front roof overhang
 - c. Vertical siding
2. Changed the proposed overhead garage and passenger doors to a style compatible with the building architecture.

Colleen Miles, applicant, is planning to attend the meeting to answer any questions.

The staff report and site plan from last month is also attached.



EXISTING ELEVATION

Agenda Item 5

CONDITIONAL USE DEV- 2015-024 & 25 American Born Salvage

Downtown Redevelopment Board

Staff Report

TO: Downtown Redevelopment Board Members

FROM: Jason Jeffries, Project Manager

DATE: April 30, 2015

APPLICANT: Ms. Colleen Miles, on behalf of Dmitri Freyman,

PROJECT: American Born Salvage; Building Rehabilitation & Site Improvements.

LOCATION: 142 South Segrave Street

REQUESTED BOARD ACTION: Review the proposed conditional use as required by Article 4, Section 7.2(b) of the 1993 Land Development Code.

Review the proposed site plan as required by Article 4, Section 8.2(c) of the 1993 Land Development Code.

Review application pursuant to the appearance standards for the redevelopment area as required by Article 3, Section 5.1 of the 1993 Land Development Code.

PROJECT DESCRIPTION:

American Born Salvage is an existing commercial building located on the west side of South Segrave Street, between West International Speedway Blvd (W ISB) and Magnolia Avenue. The applicant is proposing to establish an auto salvage and recycling facility. The 1993 Land Development Code (LDC), Article 5 (Subdivisions and Site Plans), Section 4.1, Site Plans, requires a site plan for the change of a use and for the approval of a conditional use. The proposed salvage & recycling facility is a conditional use in this location.

Note: This application was submitted prior to the effective date of the new LDC and is being reviewed based on the development standards and criteria of the former LDC.

SITE PLAN REVIEW

Article 4§8.3 of the 1993 LDC; the reviewing body, in reviewing a site plan, shall consider whether the proposal is consistent with all applicable policies of the comprehensive plan and whether any conflict in policy would be created.

Land Use and Compatibility: The site is designated Local Service Use on the City’s Future Land Use Map. The Local Service Use designation allows development with floor area ratios (FAR) up to 2. The net gross floor area of the existing building is 5,272 and the project has a FAR of 0.5. The site is a vacant light manufacturing building that is a compatible use with the adjacent commercial uses.

Adjacent Land Uses and Zoning Classifications:

	Land Uses	Land Use Designation	Zoning Classification
North	Paint Supply Store	Local Service Use	M-1 (Local Service Industry)
South	Property Maintenance Service	Local Service Use	M-1 (Local Service Industry)
East	Bus Station	Commercial Mixed Use	RDD-3 (Commercial)
West	Lumbar Yard / Building Supply	Local Service Use	M-1 (Local Service Industry)

Zoning: The current zoning is M-1 (Local Service Industry). The purpose of this district is to provide appropriate areas for the location of industrial uses, and to protect these areas from intrusion by residential and other inharmonious uses. Salvage & recycling facility is a conditional use in the M-1 zoning district. The conditional use requirement and analysis are further explained later in the report.

Requirement for Structures			
	Zone	Project	Notes
Minimum Lot Width	50'	60'	
Minimum Lot Depth	None	171.4'	
Minimum Lot Area	5,000 sq. ft.	10,455 sq. ft.	
Front Setback	None	22.9'	
Rear Setback	5'	21.7'	
Side Setback	5'	3.3'	Existing Building Condition
Max % of Lot Coverage	None	50.4%	
Building Height	None	One Story	

Site Arrangement: The site plan includes an existing warehouse building located on a 0.24-acre site. The building occupies the front portion of the site with parking in front of the building.

Parking & Traffic Flow: The project is required to provide five parking spaces. The site plan shows 5 parking spaces in front of the building requiring vehicular traffic backing into the right-of-way. Generally, the LDC prohibits parking

arrangements that require vehicular traffic to back into right-of way from private property. The applicant has provided architectural elevations with new overhead garage door on the front elevation, but the site plan does not show a drive isle to the new garage door. The site plan will need to be revised to show a drive isle to the garage door and a reduction of parking spaces to three. The applicant has also submitted a Board of Adjustment application to request a variance to reduce the required parking spaces to three and to allow backing into and/or out of a public right-of-way.

Requirement for Parking Lots			
Zone Criteria	Required	Project	Notes
3.5 spaces per 1,000 sq. ft. (office)	1		400 sq. ft.
1 space per 1,500 sq. ft. (GFA)	4		5,272 sq. ft.
TOTAL	5	3	

Site Landscaping: The proposed landscaping plan is for an existing developed site. The 1993 LDC requires sites in redevelopment areas be brought into compliance with the landscape requirements in Article 18§2 to the greatest extent possible based on the physical constraints of the site. The constraints of the site with the existing structure and pavement areas allows for lot perimeter landscaping in the front yard. The landscaping plan complies with the requirements in the LDC. Attached in this packet are the landscape plans that detail the proposed landscaping improvements.

Signage: The project is not proposing signage. Any proposed signs will have to meet the current LDC requirements.

Design Guidelines and Requirements for Redevelopment Districts

Attached are photos of the existing structure and proposed architectural elevations. The following is a summary and a staff response of the standards of Article 12§2.1 of the 1993 Land Development Code.

Project architecture: The existing building is designed in a commercial vernacular style of architecture. The applicant is proposing to preserve the building architecture. The applicant is proposing to remove the existing dock/walkway in the front of the building. A large overhead garage door will be installed to allow for the truck to enter into the building to unload materials. The building will be repainted to provide an overall unified appearance. The proposed improvements meet Redevelopment Design Standards.

Building materials: The proposed building material is wood siding. The paint colors will comply with the approved colors for Redevelopment Areas.

CONDITIONAL USE REVIEW

Article 4§7.3 of the 1993 LDC; the reviewing body, in reviewing a conditional site plan, shall consider whether the proposal fully and clearly meets all conditions established for the use.

Use Conditions: The site is located in the M-1 (Local Service Industry) zoning district. Salvage & recycling facility is a conditional use in the M-1 zoning district. Article 13§3.6 of the 1993 LDC identifies the following conditions that apply to salvage & recycling facilities.

1. All processing of salvage and/or recycled materials shall be conducted entirely within an enclosed building.
2. Outside storage of materials or equipment is prohibited.
3. All loading or unloading of materials shall occur inside a building or by dock-type facilities designed to allow loading or unloading directly into a building.
4. All loading and vehicular use areas shall consist of a durable asphalt or concrete surface which includes adequate onsite area for the stacking of trucks and trailers waiting to be loaded and unloaded.

The applicant is proposing to do all processing of auto salvaging within the existing building and will not have any outside storage of materials. The applicant is proposing loading and unloading of materials inside the building. The business should only have one truck arriving and departing at a time and stacking is not required. The project meets all the applicable conditions for salvage and recycling facilities in the M-1 zoning district.

RECOMMENDATION:

Staff recommends that the Downtown Redevelopment Board **APPROVE** the conditional use and **APPROVE** the site plan for the project known as American Born Salvage, located at 142 South Segrave Street, subject to the site being revised to include a drive isle to the new garage door, the approval of the variances by the Board of Adjustment to reduce the required parking spaces to three and permit vehicular backing into a public right-of-way, and any outstanding staff comments.

INDEX OF SHEETS	
DWG.NO.	DESCRIPTION
1	COVER
2	DEVELOPMENT INFORMATION
3	AERIAL PHOTOGRAPH OF EXISTING SITE CONDITIONS
4	DEMOLITION AND SWPPP PLAN
5	SITE LAYOUT PLAN
6	GRADING, DRAINAGE AND UTILITY PLAN
7	MISCELLANEOUS DETAILS AND NOTES
8	CITY OF DAYTONA BEACH DETAILS AND NOTES
9	CITY OF DAYTONA BEACH AS-BUILT REQUIREMENTS
10	LANDSCAPE PLAN
11	IRRIGATION PLAN

MINOR SITE PLAN MODIFICATION FOR AMERICAN BORN SALVAGE

SECTION 39, TOWNSHIP 15 S, RANGE 33 E
39-15-33-25-00-0020

142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114

FEBRUARY 18, 2015
REVISED APRIL 1, 2015

PROJECT TEAM

PROPERTY OWNER: AMERICAN BORN SALVAGE
DMITRI FREYMAN, PRESIDENT
202 MATHEW DRIVE
SANFORD, FL 32773
PHONE: (407) 314-1221
EMAIL: DIMKA1221@YAHOO.COM

AGENT: LAND DEVELOPMENT RESOURCE GROUP LLC
140 SOUTH BEACH STREET, SUITE 400
DAYTONA BEACH, FL 32114
PHONE: (386) 212-1942
EMAIL: COLLEEN@LDRGDAYTONA.COM

ENGINEER: NEWKIRK ENGINEERING, INC.
1370 NORTH US1, SUITE 204
ORMOND BEACH, FL 32174
PHONE: (386) 290-7599
EMAIL: HARRY@NEWKIRK-ENGINEERING.COM

SURVEYOR: EAST COAST LAND SURVEYING
11 COOLIGE AVENUE, SUITE-J
ORMOND BEACH, FL 32174
PHONE: (386) 672-3633 OR (386)437-0123

LANDSCAPE ARCHITECT: RICHARD L. POORE, LA
300 GATEWOOD COURT
ORMOND BEACH, FL 32174
PHONE: (386) 212-8491
EMAIL: THEPOORES@BELLSOUTH.NET

CITY APPROVAL STAMP
DEV 2015-024

LEGAL DESCRIPTION

THE SOUTHERLY 10 FEET OF LOT 1, AND ALL OF LOT 2, JAMES RIX SUBDIVISION, ACCORDING TO THE MAP OR PLAT THEREOF AS RECORDED IN MAP BOOK 4, PAGE 141, OF THE PUBLIC RECORDS OF VOLUSIA COUNTY, FLORIDA

PROJECT STATEMENT

REQUEST A CONDITIONAL USE FOR A INDOOR SALVAGE AND RECYCLING BUSINESS TO BE KNOWN AS "AMERICAN BORN SALVAGE". SITE WORK WILL CONSIST OF SITE CLEANUP, SIGNAGE, STRIPING, UTILITY UPGRADES AND LANDSCAPE CODE COMPLIANCE.

SITE DEVELOPMENT USAGE

1. SETBACK:	REQUIRED BUILDING	EXISTING BUILDING	PROPOSED BUILDING
FRONT (SEGRAVE STREET)	0 FEET	27.9 FEET	27.9 FEET
SIDE	5 FEET	3.1 FEET	3.1 FEET
REAR	5 FEET	21.7 FEET	21.7 FEET

DAYTONA BEACH PRECONSTRUCTION MEETING

NO CONSTRUCTION ON THE PROPOSED PORTIONS OF THIS PROPERTY MAY COMMENCE UNTIL A MANDATORY PRE-CONSTRUCTION MEETING IS HELD WITH THE CITY, AS STATED IN THE APPROVED DEVELOPMENT ORDER FROM THE CITY OF DAYTONA BEACH. ANY CESSATION OF CONTINUOUS ON-GOING CONSTRUCTION ON THIS PROJECT OF 90 DAYS OR MORE SHALL TRIGGER A REQUIREMENT FOR ANOTHER PRE-CONSTRUCTION MEETING TO BE HELD WITH THE CITY PRIOR TO CONTINUATION OF THE CONTINUING CONSTRUCTION.

DAYTONA BEACH REMODEL NOTE

ANY REMODEL THAT EXCEEDS 50% OF THE VALUE OF THE STRUCTURE IS A SUBSTANTIAL IMPROVEMENT AND MUST COMPLY WITH THE CITY'S FINISHED FLOOR ELEVATION REQUIREMENT OF 1 FT. ABOVE THE BFE. AS AN ALTERNATIVE TO ELEVATING THE FINISHED FLOOR OF THE BUILDING, THE BUILDING CAN BE BROUGHT INTO COMPLIANCE BY DRY-FLOOD PROOFING MEASURES. THE DESIGN MUST BE CERTIFIED BY A REGISTERED DESIGN PROFESSIONAL. IF A SUBSTANTIAL IMPROVEMENT IS PROPOSED, IF A SUBSTANTIAL IMPROVEMENT IS PROPOSED, THE ENGINEER OF RECORD (EOR) IS TO ESTABLISH THE BFE BY SIMPLIFIED METHODS.

JURISDICTIONAL AGENCY PERMIT No.

CITY OF DAYTONA BEACH (SITE PLAN MODIFICATION) DEV2015-024
CITY OF DAYTONA BEACH (CONDITIONAL USE) DEV2015-025

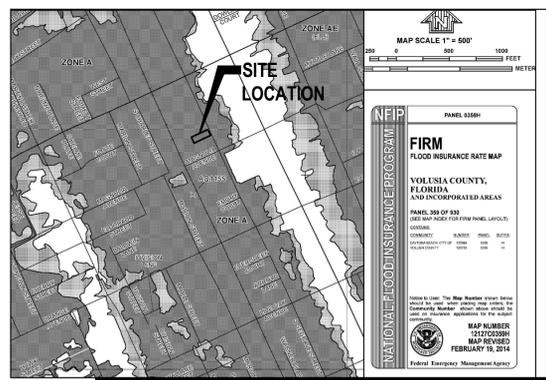
THE GENERAL CONTRACTOR SHALL ENSURE THAT ANY SUBCONTRACTOR HAS A COMPLETE SET OF CONSTRUCTION DRAWINGS FOR ITS RESPECTIVE WORK. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR SUBCONTRACTORS ONLY UTILIZING INDIVIDUAL DRAWINGS FOR ITS WORK WHERE ADDITIONAL INFORMATION MAY BE CONTAINED ON OTHER DRAWINGS WITHIN THE SET.

THESE DRAWINGS ARE THE PROPERTY OF NEWKIRK ENGINEERING, INC. ANY USE OR REPRODUCTION IN WHOLE OR PART IS PROHIBITED WITHOUT THE EXPRESSED WRITTEN CONSENT OF NEWKIRK ENGINEERING, INC. COPYRIGHT 2013 ALL RIGHTS RESERVED.



LOCATION MAP

SCALE: 1" = 500'



FLOOD ZONE MAP

SCALE: 1" = 800'

FLOOD ZONES "A" AND "X"



ZONING MAP

SCALE: 1" = 500'

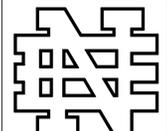
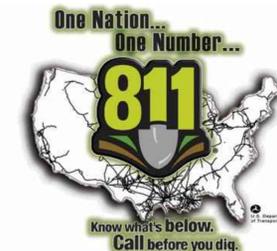
ZONING DISTRICT:
M1 - LOCAL SERVICE INDUSTRY



SOILS MAP

SCALE: 1" = 400'

SOIL TYPE: (71) URBAN LAND

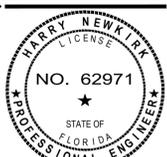


NEWKIRK
ENGINEERING

1370 North US1, Suite 204
Ormond Beach, Florida 32174
Phone (386) 290-7599
Harry@Newkirk-Engineering.com

Certificate of Authorization
No. 30209
© 2013

Civil Engineering
Land Development
Construction Engineering &
Inspection



NOT VALID UNLESS SIGNED AND SEALED

DRAWING NUMBER

1

GENERAL CONSTRUCTION NOTES

- GOVERNING SPECIFICATIONS: CITY OF DAYTONA BEACH LAND DEVELOPMENT CODE, CITY OF DAYTONA BEACH STANDARD CONSTRUCTION DETAILS AND SPECIFICATIONS, LATEST EDITION, FLORIDA DEPARTMENT OF TRANSPORTATION ROADWAY AND TRAFFIC DESIGN AND CONSTRUCTION STANDARDS, LATEST EDITION, AND STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS, LATEST EDITION, AND SUPPLEMENTS THERETO IF NOTED IN THE SPECIAL PROVISIONS FOR THIS PROJECT.
- THE CONTRACTOR SHALL PAY FOR AND OBTAIN A BUILDING PERMIT. THE ENGINEER WILL SCHEDULE THE PRECONSTRUCTION CONFERENCE BEFORE THE CONTRACTOR'S START OF WORK. THE CONTRACTOR SHALL CONTACT THE PERMITS AND LICENSING DEPARTMENT AT (386) 671-8140 FOR INFORMATION ON ISSUANCE OF CITY PERMITS AND / OR OTHER REQUIREMENTS.
- IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY THE ENGINEER OF ANY DEFICIENCIES OR DISCREPANCIES AMONG THE DIVISIONS OF THE DRAWING AND SPECIFICATIONS PRIOR TO THE BID DATE. NEITHER THE OWNER OR ENGINEER WILL BE RESPONSIBLE FOR ANY DEFICIENCIES OR DISCREPANCIES RAISED AFTER THE BID OPENING. ACCORDINGLY, IN LIGHT OF THESE OBLIGATIONS, THE ENGINEER IS OBLIGATED TO INTERPRET THE DRAWINGS AND SPECIFICATIONS IN A MANNER THAT WILL PROVIDE THE OWNER WITH A COMPLETE, FUNCTIONING FACILITY FOR THE BID PRICE.
- THESE DRAWINGS AND THE PROJECT MANUAL ARE COMPLEMENTARY, AND ANY REQUIREMENT OF ONE SHALL BE A REQUIREMENT OF THE OTHER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CAREFULLY EXAMINE THE DRAWINGS AND SPECIFICATIONS AND TO COMPARE THE REQUIREMENTS OF EACH DIVISION AND ENSURE THAT EACH TRADE OR SUBCONTRACTOR IS MAKING THE ALLOWANCES NECESSARY TO PROVIDE THE OWNER A COMPLETE FACILITY, OPERATIONAL IN ALL RESPECTS, UNLESS OTHERWISE SPECIFICALLY STATED IN THE DRAWINGS.
- THE ENGINEER ASSUMES NO RESPONSIBILITY FOR INSTRUCTING THE CONTRACTOR IN THE METHODS OF CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE METHOD TO CONSTRUCT THE IMPROVEMENTS AS SHOWN ON THE PLANS.
- ONLY ONE TEMPORARY CONSTRUCTION SIGN IS PERMITTED. NOT TO EXCEED 32 SQUARE FEET IN SIGN AREA, MAXIMUM HEIGHT OF 8 FEET AND NO CLOSER THAN 10 FT FROM PUBLIC RIGHT-OF-WAY. THE CONTRACTOR SHALL APPLY FOR A TEMPORARY SIGN PERMIT AT THE DAYTONA BEACH BUILDING DEPARTMENT. THE SIGN MUST BE REMOVED UPON RECEIPT OF THE CERTIFICATE OF OCCUPANCY.
- CONSTRUCTION NOISE IS RESTRICTED TO LESS THAN 86 dBA BETWEEN THE HOURS OF 7 AM AND 10 PM AND LESS THAN 55 dBA BETWEEN THE HOURS OF 10 PM AND 7 AM DAILY.
- NIGHT TIME CONSTRUCTION LIGHTING SHALL BE RESTRICTED TO NO MORE THAN 0.5 FT-CANDLE AT THE PROPERTY BOUNDARY.
- LITTER CONTROL MEASURES TO PREVENT WIND-DRIVEN DEBRIS SHALL BE IMPLEMENTED THROUGHOUT THE DURATION OF CONSTRUCTION. ALL DEBRIS SHALL BE REMOVED AND THE PROJECT SITE CLEANED WITHIN 30 DAYS OF COMPLETION OF CONSTRUCTION.
- AT NO TIME SHALL EXCAVATIONS BE LEFT UNCOVERED AFTER WORKING HOURS. CONTRACTOR SHALL SECURE THE WORK AREA AT THE END OF EACH DAY'S WORK.
- AT ALL TIMES, THE CONTRACTOR SHALL TAKE PRECAUTIONS TO PROTECT UNDERGROUND UTILITIES, STRUCTURES AND OTHER ASSOCIATED FACILITIES FROM DAMAGE DURING CONSTRUCTION. IT IS THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE MEASURES OF PROTECTION. ANY DAMAGED FACILITIES SHALL BE REPAIRED OR REPLACED AS DIRECTED BY THE CITY OR ENGINEER AT THE CONTRACTORS EXPENSE.
- THERE SHALL BE NO DEVIATIONS FROM THESE PLANS UNLESS APPROVED IN WRITING BY THE ENGINEER AND THE OWNER.
- THE CONTRACTOR SHALL CONTACT ALL CONCERNED UTILITIES AT LEAST FORTY-EIGHT (48) HOURS IN ADVANCE OF CONSTRUCTION OPERATIONS.
- CONTRACTOR SHALL COORDINATE AND COMPLY WITH ALL UTILITY COMPANIES INVOLVED IN PROJECT AND PAY ALL REQUIRED FEES AND COST.
- FILL MATERIAL SHALL CONSIST OF "CLEAN", FINE SAND WITH LESS THAN 5 PERCENT SOIL FINES.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING CERTIFIED MATERIAL TEST RESULTS TO THE ENGINEER OF THE RECORD PRIOR TO THE RELEASE OF FINAL CERTIFICATION BY THE ENGINEER. TEST RESULTS MUST INCLUDE, BUT MAY NOT BE LIMITED TO, DENSITIES FOR SUBGRADE AND BASE DENSITIES AT UTILITY CROSSINGS, MANHOLES, INLETS, STRUCTURES. TEST SHALL INCLUDE ASPHALT GRADATION REPORTS, CONCRETE CYLINDERS, ETC.
- WHERE NEW ASPHALT MEETS EXISTING ASPHALT, THE EXISTING ASPHALT SHALL BE SAW CUT TO PROVIDE A STRAIGHT EVEN LINE.
- PRIOR TO REMOVING CURB OR GUTTER, THE ADJACENT ASPHALT SHALL BE SAW CUT TO PROVIDE A STRAIGHT EVEN LINE.
- ALL PROPOSED ELEVATIONS REFER TO FINISHED GRADES.
- CONCRETE WALKS SHALL BE 4 INCHES THICK HAVING A 3,000 PSI STRENGTH, POURED OVER PROPERLY PREPARED SUBGRADE. ALL CONCRETE SIDEWALKS SHALL BE 9 INCHES THICK ACROSS DRIVEWAYS. 1/2 INCH EXPANSION JOINTS SHALL BE PLACED AT A MAXIMUM OF 50'. CRACK CONTROL JOINTS SHALL BE 5' ON CENTERS.
- CORE TESTS SHALL BE TAKEN TO VERIFY THICKNESS AND SUBSURFACE COMPACTION. PROVIDE FOR THREE SAMPLES, RANDOMLY LOCATED. TEST FOR EXTRACTION, GRADATION, LABORATORY DENSITY, AND MARSHALL'S STABILITY. PROVIDE A CERTIFICATE FROM THE TESTING AGENCY THAT MATERIALS AND INSTALLATION COMPLY WITH SPECIFICATIONS, SIGNED BY THE ASPHALTIC CONCRETE PRODUCER AND CONTRACTOR. ALL COSTS OF TESTS SHALL BE PAID BY THE CONTRACTOR. IF TESTS SHOW THE INSTALLATION DOES NOT MEET SPECIFICATIONS, THE PAVING SHALL BE REMOVED, REPLACED, AND RETESTED.
- IF ANY MUCK-LIKE MATERIAL IS DISCOVERED, IT WILL BE REQUIRED TO BE REMOVED, BACKFILLED WITH APPROPRIATE FILL, COMPACTED, AND TESTED USING AASHTO T-180 MODIFIED PROCTOR METHOD.
- FILL MATERIAL IS TO BE PLACED IN ONE FOOT LIFTS AND COMPACTED TO THE APPROPRIATE DENSITY (98% FOR PAVED AREAS AND 95% FOR BUILDING PADS AND ALL OTHER AREAS AS PER AASHTO T-180).
- NO BURYING OF ANY ORGANIC MATERIALS ALLOWED.

SITE AND GENERAL INFORMATION

- THE PROPERTY AREA BOUNDARY CONSISTS OF 0.240 ACRES. FOR BOUNDARY AND TOPOGRAPHIC SURVEY REFER TO THE SURVEY PERFORMED BY EAST COAST LAND SURVEYING, PROJECT "142 SEGRAVE STREET SOUTH".
- THE CURRENT ZONING IS M1 - LOCAL SERVICE INDUSTRY.
- THE TAX PARCEL NUMBER IS 39-15-25-00-0020.
- THE EXISTING SITE CONDITION IS A VACANT INDUSTRIAL SITE. THE FLUCFCS LAND USE IS 150 (INDUSTRIAL).
- PER THE USDA NATURAL RESOURCES CONSERVATION SERVICE FOR VOLUSIA COUNTY, THE SCS SOILS MAP INDICATES THE SITE CONSISTS OF 71 (URBAN LAND).
- THE SITE IS LOCATED WITHIN ZONE "A" PER MAP PANEL No. 12127C0359H, DATED FEBRUARY 19, 2014. THE ELEVATION OF THE FLOOD ZONE IS ELEVATION 5.0 N.A.V.D. PER CONTOUR INTERPOLATION OF FEMA FIRM AND VOLUSIA COUNTY LIDAR MAPS.
- ELECTRICAL UTILITY SERVICE WILL BE PROVIDED BY FLORIDA POWER & LIGHT. NATURAL GAS WILL BE PROVIDED BY TECO PEOPLES GAS COMPANY. TELEPHONE, CABLE AND INTERNET SERVICE WILL BE PROVIDED BY AT&T. CABLE TV AND INTERNET CAN ALSO BE PROVIDED BY BRIGHTHOUSE NETWORKS.
- SOLID WASTE WILL BE COLLECTED AND DISPOSED OF BY WASTE MANAGEMENT, INC.
- THE SITE IS NOT LOCATED WITHIN THE LIMITS OF A WELLHEAD PROTECTION ZONE AND THERE IS NO ORDINARY HIGH WATER (OHW) LINE WITHIN THE SITE.
- STORMWATER WILL CONTINUE TO SHEET FLOW TO SEGRAVE STREET RIGHT-OF-WAY.
- POTABLE WATER AND WASTEWATER UTILITY SERVICE WILL BE PROVIDED BY CITY OF DAYTONA BEACH.
- IRRIGATION SERVICE WILL BE PROVIDED BY CITY OF DAYTONA BEACH IRRIGATION METER.
- ALL ON-SITE WATER AND SEWER FACILITIES ARE OWNED AND MAINTAINED BY THE PROPERTY OWNER.
- THERE WILL BE NO HAZARDOUS MATERIALS STORED ON-SITE.
- THERE WILL BE NO UNDERGROUND OR ABOVE GROUND FUEL OR CHEMICAL STORAGE TANKS ON-SITE.

LEGEND

NOTE: NOT ALL SYMBOLS SHOWN HERE MAY BE APPLICABLE TO THESE DRAWINGS, ALSO THERE MAY BE ADDITIONAL SYMBOLS WITHIN PLANS NOT SHOWN HERE, SEE INDIVIDUAL DRAWING LEGEND WHERE APPLICABLE.

#3	BENCHMARK ID	■	FOUND 4" BY 4" CONCRETE MONUMENT
B24	BORING ID	— FOC —	EXISTING EASEMENT
□	EXISTING CABLE TV PEDESTAL	— FFW —	EXISTING UNDERGROUND FIBER OPTIC CABLE
c	EXISTING CAP OR PLUG	— GAS —	EXISTING FORCE MAIN (# INDICATES SIZE)
⊙	EXISTING CLEAN OUT	— OHE —	EXISTING GAS MAIN
⊗	EXISTING CONDUIT RISER/ MARKER	— OHT —	EXISTING OVERHEAD ELECTRIC CABLES
□	EXISTING ELECTRIC METER	— ORAW —	EXISTING OVERHEAD TRAFFIC SIGNAL CABLE
11.7	EXISTING ELEVATION (SOFT)	— RREC —	EXISTING RAW WATER MAIN (# INDICATES SIZE)
5.0±	PROPOSED ELEVATION (SOFT)	— RSEC —	EXISTING RECLAIM WATER MAIN (# INDICATES SIZE)
11.75	EXISTING ELEVATION (HARD)	— RSW —	PROPOSED SANITARY SEWER (# INDICATES SIZE)
5.00	PROPOSED ELEVATION (HARD)	— RW —	PROPOSED WATER MAIN (# INDICATES SIZE)
☼	EXISTING FIRE HYDRANT	12	EXISTING CONTOUR
☼	PROPOSED FIRE HYDRANT	10	PROPOSED CONTOUR (SOFT)
↔	EXISTING FLOW DIRECTION	10	PROPOSED CONTOUR (HARD)
↔	PROPOSED FLOW DIRECTION	UTEL	EXISTING UNDERGROUND TELEPHONE CABLE
⊗	EXISTING GAS METER	UTV	EXISTING UNDERGROUND TELEVISION CABLE
⊗	EXISTING GAS VALVE	UGE	
— · · · —	EXISTING GUY WIRE & ANCHOR PIN	— · · · —	JURISDICTIONAL WETLAND LINE
□	EXISTING MAIL BOX	— BSAN —	EXISTING SANITARY SEWER (# INDICATES SIZE)
○	EXISTING MANHOLE (UNKNOWN)	— BAW —	EXISTING WATER MAIN (# INDICATES SIZE)
⊙	PROPOSED MANHOLE	— — — —	EXISTING PIPE OR CONDUIT (TYPE SPECIFIED)
⊙	EXISTING SANITARY SEWER CLEANOUT	— · · · —	EXISTING SWALE OR CENTER OF DITCH
⊙	EXISTING SANITARY SEWER MANHOLE	— · · · —	PROPOSED SWALE OR CENTER OF DITCH
— — —	EXISTING ROAD SIGNS AND POSTS	— — — —	EXISTING TOP OF DITCH BANK
— —	PROPOSED SIGN AND POST	— · · —	EXISTING BOTTOM OF DITCH BANK
— — — —	EXISTING TEE	— x — x —	EXISTING WOOD FENCE
— —	EXISTING UTILITY POLE	— x — x —	EXISTING WIRE OR CHAIN LINK FENCE
⊗	EXISTING VALVE IRRIGATION	— x — x —	PROPOSED WIRE OR CHAIN LINK FENCE
⊗	EXISTING VALVE WATER	— □ — □ —	PROPOSED SILT/SEDIMENT FENCE
⊗	PROPOSED WATER VALVE	~ ~ ~ ~ ~	PROPOSED COIR ROLL OR WATTLE
⊗	EXISTING WATER METER	— ○ — ○ —	PROPOSED FLOATING TURBIDITY BARRIER
— — —	EXISTING STORM SEWER WITH INLET	— TP — TP —	PROPOSED TREE PROTECTION
— — —	PROPOSED STORM SEWER WITH INLET		
●	FOUND IRON ROD		

STATE OF FLORIDA DEPARTMENT OF TRANSPORTATION ROADWAY AND TRAFFIC DESIGN STANDARDS - 2015 AND QUALIFIED PRODUCTS LIST

INDEX NO.	DESCRIPTION	INDEX NO.	DESCRIPTION
102	TEMPORARY EROSION AND SEDIMENT CONTROL	601	TWO-LANE, TWO-WAY, WORK OUTSIDE SHOULDER
300	CURB & CURB AND GUTTER	602	TWO-LANE, TWO-WAY, WORK ON SHOULDER
304	PUBLIC SIDEWALK CURB RAMPS	660	PEDESTRIAN CONTROL FOR CLOSURE OF SIDEWALKS
305	CONCRETE PAVEMENT JOINTS	860	ALUMINUM PEDESTRIAN / BICYCLE PICKET RAILING
310	CONCRETE SIDEWALK	17302	TYPICAL SECTIONS FOR PLACEMENT OF SINGLE & MULTIPLE-COLUMN SIGNS
515	TURNOUT	17346	SPECIAL MARKING AREAS
600	GENERAL INFORMATION FOR TRAFFIC CONTROL THROUGH WORK ZONES		

ABBREVIATIONS

AWWA WORKS	AMERICAN WATER ASSOCIATION	H/C	HANDICAP	RCP	REINFORCED CONCRETE PIPE
CMP	CORRUGATED METAL PIPE	HDPE	HIGH DENSITY POLYETHYLENE	RPM	REFLECTIVE PAVEMENT MARKER
CPP	CORRUGATED PLASTIC PIPE	INV	INVERT	R/W	RIGHT-OF-WAY
CTV	CABLE TELEVISION	K _t	HORIZONTAL PERMEABILITY	SAN	SANITARY
DIP	DUCTILE IRON PIPE EASEMENT	K _v	VERTICAL PERMEABILITY	SH	SEASONAL HIGH
ESMT	EXISTING	KO	KNOCK OUT	SMH	SANITARY MANHOLE
EXIST	EXISTING	LF	LINEAL FEET	SS	SANITARY SEWER
FAC	FLORIDA ADMINISTRATIVE CODE	MB	MAP BOOK	SWPPP	STORMWATER POLLUTION PREVENTION PLAN
FDEP	FLORIDA DEPARTMENT OF ENVIRONMENTAL PROTECTION	MES	MITERED END SECTION	TSB	TEMPORARY SEDIMENT BASIN
FEMA	FEDERAL EMERGENCY MANAGEMENT AGENCY	MJ	MECHANICAL JOINT	TYP	TYPICAL
FH	FIRE HYDRANT	N/A	NOT APPLICABLE	UGE	UNDERGROUND ELECTRIC
FOC	FIBER OPTIC CABLE	NIC	NOT IN CONTRACT	UGT	UNDERGROUND TELEPHONE
FF EL	FINISH FLOOR ELEVATION	NGVD	NATIONAL GEODETIC VERTICAL DATUM	USACOE	UNITED STATES ARMY CORP OF ENGINEERS
FM	FORCE MAIN	OHE	OVERHEAD ELECTRIC	W	WATER (POTABLE)
FPD	FEET PER DAY	OR	OFFICIAL RECORD		
G	GAS	PG	PAGE		
GW	GROUND WATER	PSI	POUNDS PER SQUARE INCH		
		PVC	POLYVINYL CHLORIDE		
		PVMT	PAVEMENT		
		R	RADIUS		

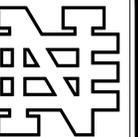
SITE DEVELOPMENT USAGE

1. SETBACK:	REQUIRED BUILDING	EXISTING BUILDING	PROPOSED BUILDING
FRONT (SEAGRAVE STREET)	0 FEET	27.9 FEET	27.9 FEET
SIDE	5 FEET	3.1 FEET	3.1 FEET
REAR	5 FEET	21.7 FEET	21.7 FEET
2. EXISTING SITE COVERAGE:	SQ. FT	ACRE	%
BUILDING	5,272	0.121	50.4
PAVEMENT, CONCRETE, PADS	1,504	0.035	14.4
OPEN SPACE	3,679	0.084	35.2
TOTAL SITE	10,455	0.240	100.0
TOTAL IMPERVIOUS	6,776	0.156	64.8
FLOOR AREA RATIO (FAR)	0.504		
3. PROPOSED SITE COVERAGE:	SQ. FT	ACRE	%
BUILDING	5,272	0.121	50.4
PAVEMENT, CONCRETE, PADS	1,451	0.033	13.9
OPEN SPACE	3,732	0.086	35.7
TOTAL SITE	10,455	0.240	100.0
TOTAL IMPERVIOUS	6,723	0.154	64.3
4. STORMWATER UTILITY FEE AREA TABLE:	SQ. FT	ACRE	
PRE DEVELOPMENT IMPERVIOUS SURFACE	6,776	0.156	
POST DEVELOPMENT IMPERVIOUS SURFACE	6,723	0.154	
DIFFERENCE	53	0.002	
5. PARKING REQUIRED:			
3.5 SPC PER 1,000 SF (OFFICE) X 400 SF	=	1 SPACE	
1 SPC PER 1,500 SF (WAREHOUSE) X 4,872	=	4 SPACES	
3 SPC PER BAY X 1 BAY	=	3 SPACES	
PARKING REQUIRED	=	8 SPACES	
6. OVERALL PARKING PROVIDED:			
HANDICAP STANDARD	1 SPACE		
	3 SPACES		
TOTAL	4 SPACES		

REVISIONS

DATE	DESCRIPTION
04/01/15	PER CITY COMMENTS

CITY APPROVAL STAMP
DEV 2015-024



NEWKIRK ENGINEERING

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DEVELOPMENT INFORMATION
AMERICAN BORN SALVAGE
 142 SOUTH SEGRAVE STREET
 DAYTONA BEACH, FL 32114



PROJECT No: 2015-21

DATE: FEBRUARY 2015

DESIGN BY: DAB

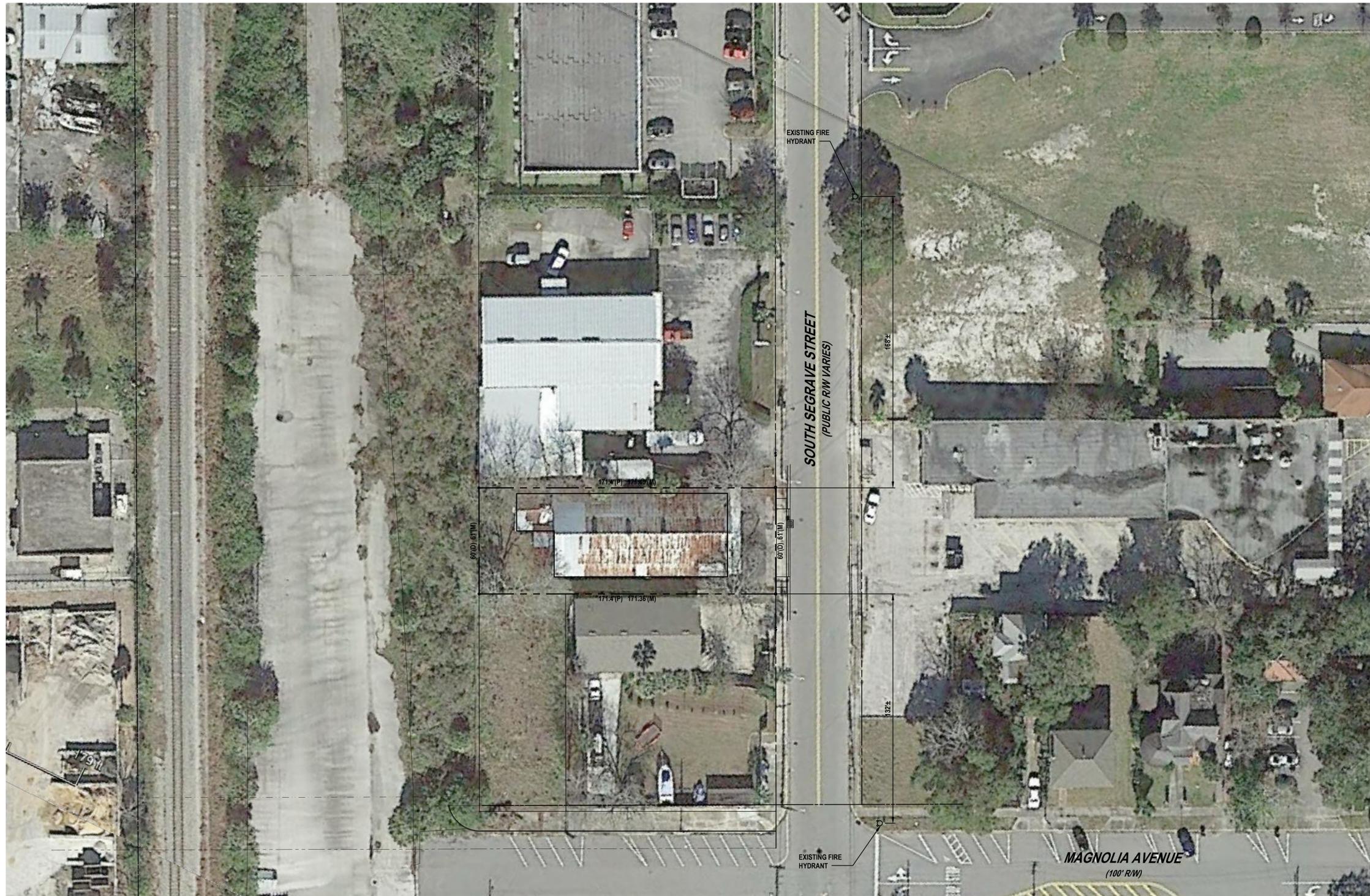
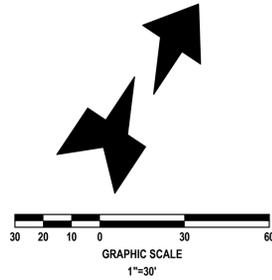
DRAWN BY: HHN

CHECKED BY: HHN

SCALE: AS SHOWN

DRAWING NUMBER

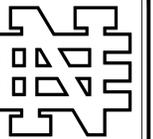
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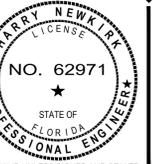
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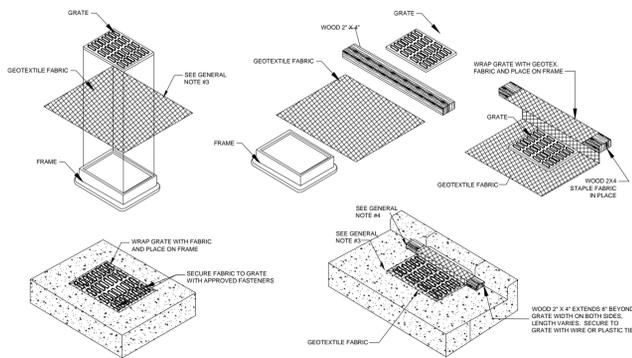
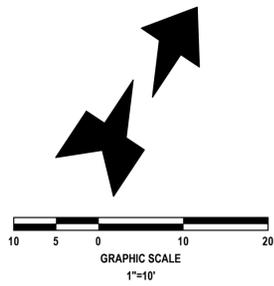
**AERIAL PHOTOGRAPH OF
EXISTING SITE CONDITIONS
AMERICAN BORN SALVAGE**
142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114



PROJECT No: 2015-21
DATE: FEBRUARY 2015
DESIGN BY: DAB
DRAWN BY: HHN
CHECKED BY: HHN
SCALE: 1" = 30'

DRAWING NUMBER

3



YARD INLET PROTECTION
(CAN BE INSTALLED IN ANY INLET WITHOUT A CURB BOX)

CURB INLET PROTECTION

GENERAL NOTES:

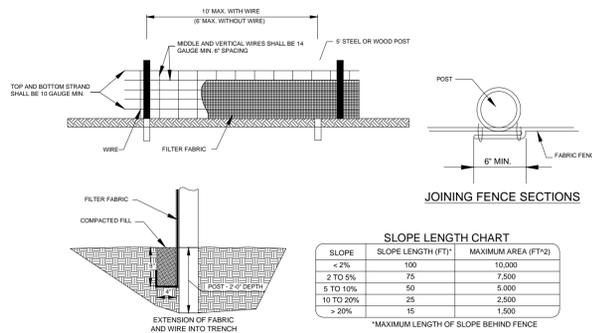
1. GEOTEXTILE TO BE MIRAFI FILTERWEAVE 402 OR GEOTEX 111F. ALTERNATIVES INCLUDE APPROVED EQUAL ASTM D4491 OR 100 TO 150 GALLON PER MINUTE PER SQUARE FOOT.
2. WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL INTO THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.
3. FINISHED SIZE, INCLUDING FLAP POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10' AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.
4. FOR CURB INLET PROTECTION AN ADDITIONAL 18" OF FABRIC IS WRAPPED AROUND THE WOOD AND SECURED WITH STAPLES. THE WOOD SHALL NOT BLOCK THE ENTIRE HEIGHT OF THE CURB BOX OPENING.
5. TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.
6. THE CONTRACTOR SHALL DEMONSTRATE A METHOD OF MAINTENANCE, USING A SEWN FLAP, HAND HOLDS, OR OTHER METHOD TO PREVENT ACCUMULATED SEDIMENT FROM ENTERING THE INLET.

GEOTEXTILE FABRIC INLET PROTECTION

(NOT TO SCALE)

SILT FENCE DETAIL

(NOT TO SCALE)



NOTES:

1. WWM MINIMUM 32" WIDTH WITH A MINIMUM OF 6 LINE WIRES WITH 12" STAY SPACING.
2. FILTER FABRIC SHALL BE 36" WIDE AND SHALL BE FASTENED ADEQUATELY TO THE WIRE.
3. STEEL POST SHALL BE 5'-0" IN LENGTH AND BE OF THE SELF-FASTENER ANGLE STEEL TYPE.
4. WOOD POST SHALL BE 5'-0" IN LENGTH AND 2" IN DIAMETER.
5. SUPPORT POSTS SHALL BE INSTALLED ON THE DOWNHILL SIDE OF THE SEDIMENT FENCE (DOWNSTREAM FROM EXPECTED FLOWS).
6. ACCUMULATED SEDIMENT SHALL BE REMOVED WHEN 25% THE HEIGHT OF FENCE.
7. MAXIMUM DRAINAGE AREA 10,000 SF PER 100' OF FENCE.
8. MAXIMUM LENGTH UPSLOPE FROM FENCE PER CHART

LEGEND:

- LIMITS OF DEVELOPMENT (PROPERTY LINE)
- APPROXIMATE LIMITS OF ASPHALT, CONCRETE PAVEMENT (1,279 ± SF)
- APPROXIMATE SIDEWALK REMOVAL (362 SF)
- APPROXIMATE RAMP AND STAIR REMOVAL (203 SF)
- PROPOSED TYPE III SILT FENCE

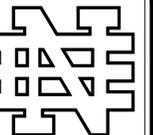
DEMOLITION AND SWPPP NOTES:

1. ALL EROSION AND SEDIMENT CONTROL DEVICES AND VISIBLE BARRICADES SHALL BE INSTALLED AND APPROVED BY THE ENGINEER PRIOR TO THE START OF DEMOLITION AND CLEARING AND GRUBBING AND SHALL REMAIN IN PLACE UNTIL ALL WORK IS COMPLETE.
2. THE CONTRACTOR SHALL SCHEDULE A PRECONSTRUCTION MEETING WITH CITY AND ENGINEER PRIOR TO START OF CONSTRUCTION.
3. THE SITE SHALL BE CLEARED AND GRUBBED OF ALL VEGETATION AND DEBRIS WITHIN THE APPROXIMATE LIMITS OF CLEARING AS INDICATED ON THE DRAWING AND HAULED TO AN APPROVED LANDFILL OR RECYCLED.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING CITY OF DAYTONA BEACH UTILITIES, FLORIDA SUNSHINE AND EMERGENCY SERVICES FOR RESPECTIVE UTILITY LOCATIONS AND "NOTICE" OF CONSTRUCTION ACTIVITY.
5. ALL OFF-SITE AREAS DISTURBED DURING CONSTRUCTION ACTIVITY SHALL BE IMMEDIATELY RESTORED TO PRIOR CONDITION UPON COMPLETION OF WORK.
6. SOILS ARE TO BE STABILIZED BY WATER OR OTHER MEANS DURING CONSTRUCTION. THIS IS INTENDED TO REDUCE SOIL EROSION, BLOWING SAND AND THE IMPACT TO NEIGHBORING PROPERTIES.
7. MAINTAIN EROSION CONTROL MEASURES AFTER EACH RAIN AND AT LEAST ONCE A WEEK.

CITY APPROVAL STAMP
DEV 2015-024

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DEMOLITION AND SWPPP PLAN
AMERICAN BORN SALVAGE
142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114



NOT VALID UNLESS SIGNED AND SEALED

PROJECT No:	2015-21
DATE:	FEBRUARY 2015
DESIGN BY:	DAB
DRAWN BY:	HHN
CHECKED BY:	HHN
SCALE:	1" = 10'

DRAWING NUMBER

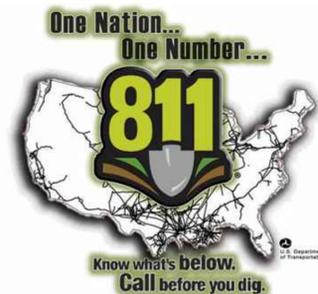
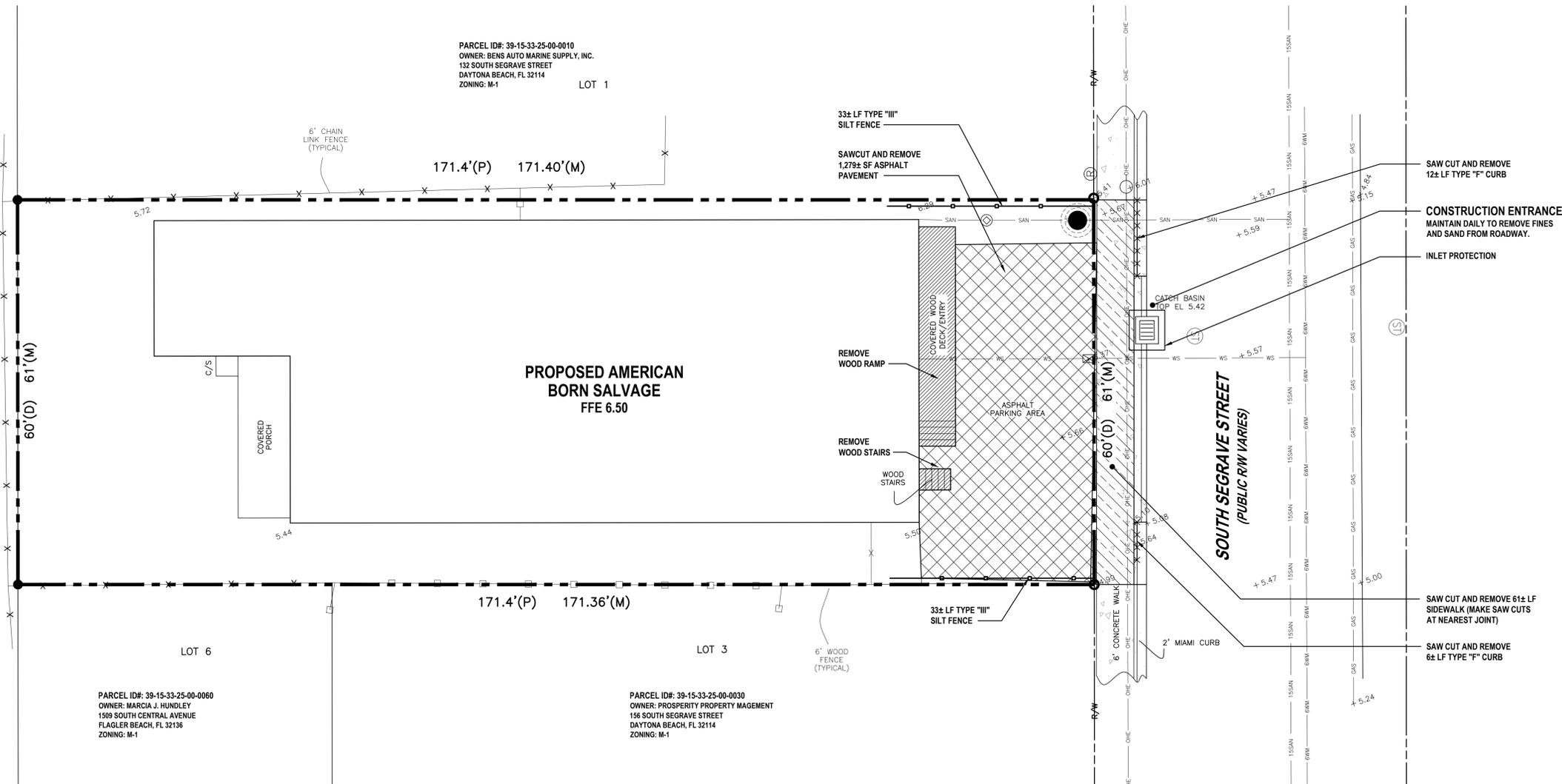
4

PARCEL ID#: 39-15-33-25-00-0010
OWNER: BENS AUTO MARINE SUPPLY, INC.
132 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114
ZONING: M-1 LOT 1

PARCEL ID#: 39-15-33-25-24-0010
OWNER: ROSEN MATERIALS LLC
1371 SAWGRASS CORPORATE PARKWAY
SUNRISE, FL 33323
ZONING: M-1

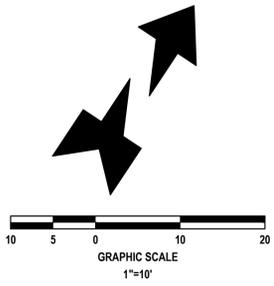
PARCEL ID#: 39-15-33-25-00-0060
OWNER: MARCIA J. HUNDLEY
1509 SOUTH CENTRAL AVENUE
FLAGLER BEACH, FL 32136
ZONING: M-1

PARCEL ID#: 39-15-33-25-00-0030
OWNER: PROSPERITY PROPERTY MAGEMENT
156 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114
ZONING: M-1



WARNING !!

CONTRACTOR SHALL TAKE ALL PRECAUTIONS DURING CONSTRUCTION TO AVOID CONTACT WITH EXISTING UNDERGROUND UTILITIES, GAS MAINS AND OVERHEAD ELECTRIC IN THE RIGHT-OF-WAY.



GENERAL WATER NOTES:

1. THE CITY OF DAYTONA BEACH UTILITY DEPARTMENT SHALL BE NOTIFIED PRIOR TO BEGINNING ANY WATER SYSTEM CONSTRUCTION.
2. ALL WATER DISTRIBUTION CONSTRUCTION SHALL BE IN ACCORDANCE WITH FDEP REGULATIONS AND THE CITY OF DAYTONA BEACH UTILITY DETAILS AND SPECIFICATIONS (LATEST EDITION).
3. CONTRACTOR IS TO VERIFY THE LOCATION (HORIZONTAL AND VERTICAL) OF ALL EXISTING UTILITIES (UNDERGROUND AND OVERHEAD) PRIOR TO COMMENCEMENT OF CONSTRUCTION. ANY CONFLICTS SHALL BE REPORTED TO THE ENGINEER.
4. CONTRACTOR IS RESPONSIBLE FOR RESTORATION OF ANY EXISTING PAVEMENT, SIDEWALK, CURBING, UTILITIES AND DRAINAGE SYSTEMS DAMAGED DURING CONSTRUCTION. ALL DAMAGED AREAS TO BE RESTORED TO ORIGINAL CONDITION OR BETTER.
5. THE CONTRACTOR SHALL MAKE ALL ATTEMPTS TO LOCATE BURIED UTILITIES AND NOTIFY THE UTILITY COMPANIES 48 HOURS PRIOR TO CONSTRUCTION.
6. BACKFLOW PREVENTER DEVICE IS TO BE INSTALLED AND CERTIFIED BY THE PLUMBING CONTRACTOR.

GENERAL SEWER NOTES:

1. CITY OF DAYTONA BEACH SHALL BE NOTIFIED PRIOR TO ANY SEWER CONSTRUCTION.
2. ALL SEWER CONSTRUCTION MUST COMPLY WITH FDEP REGULATIONS AND THE CITY OF DAYTONA BEACH UTILITY DETAILS AND SPECIFICATIONS (LATEST EDITION).
3. SEWER CONSTRUCTION SHALL BE ACCOMPLISHED BY THE USE OF A LASER INSTRUMENT UNLESS ANOTHER METHOD IS PREVIOUSLY APPROVED BY CITY OF DAYTONA BEACH.
4. THE CONTRACTOR SHALL AT ALL TIMES, DEWATER THE GROUND SUFFICIENTLY TO KEEP THE GROUNDWATER ELEVATION A MINIMUM OF 6" BELOW THE PIPE BEING LAID WITHIN THE AREA OF THE TRENCH.
5. EXCAVATION AND BACKFILL: THE CONTRACTOR SHALL PROVIDE ADEQUATE SHEETING AND BRACING OF EXCAVATION WORK IN ORDER TO PROVIDE FOR THE SAFETY OF WORKMEN, AS WELL AS REPRESENTATIVES OF THE CITY, THE DESIGN ENGINEER, AND THE DEVELOPER.
6. ALL TRENCHES SHALL BE BACKFILLED WITH ACCEPTABLE MATERIAL AND COMPACTED TO MINIMUM COMPACTION OF (98) PERCENT OF THE OPTIMUM DENSITY OF THAT MATERIAL BASED ON THE AASHTO T-100 MODIFIED PROCTOR TEST.
7. ALL TESTING REQUIRED BY THE CITY SHALL BE PAID FOR BY THE CONTRACTOR.

GENERAL GRADING NOTES:

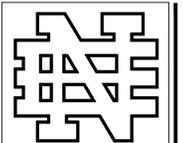
1. CROSS SLOPES OF ACCESSIBLE PARKING SPACES AND ACCESS AISLE SHALL NOT EXCEED 1:48 IN ACCORDANCE WITH SECTION 502.4 OF FACBC.
2. THE MAXIMUM ELEVATION CHANGE AT THE ENTRANCE DOOR MAY NOT EXCEED 1/2 INCH.
3. ALL SIDEWALK LANDINGS SHALL HAVE SLOPES NO GREATER THAN 1/8 INCH / 1 FOOT IN MANEUVERING SPACES AT ALL DOORWAYS.
4. ALL SIDEWALKS SHALL HAVE A 1 PERCENT CROSS SLOPE.
5. ALL IMPROVEMENTS SHALL BE STAKED FOR CONSTRUCTION BY MEANS OF DIGITAL COORDINATES BY SURVEY OR UTILIZING GEODETTIC TOTAL STATION. SCALING OF DRAWINGS FOR PURPOSES OF STAKING ARE AT THE SURVEYOR.

CITY APPROVAL STAMP
DEV 2015-024



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DATE	DESCRIPTION
04/01/15	PER CITY COMMENTS



NEWKIRK ENGINEERING

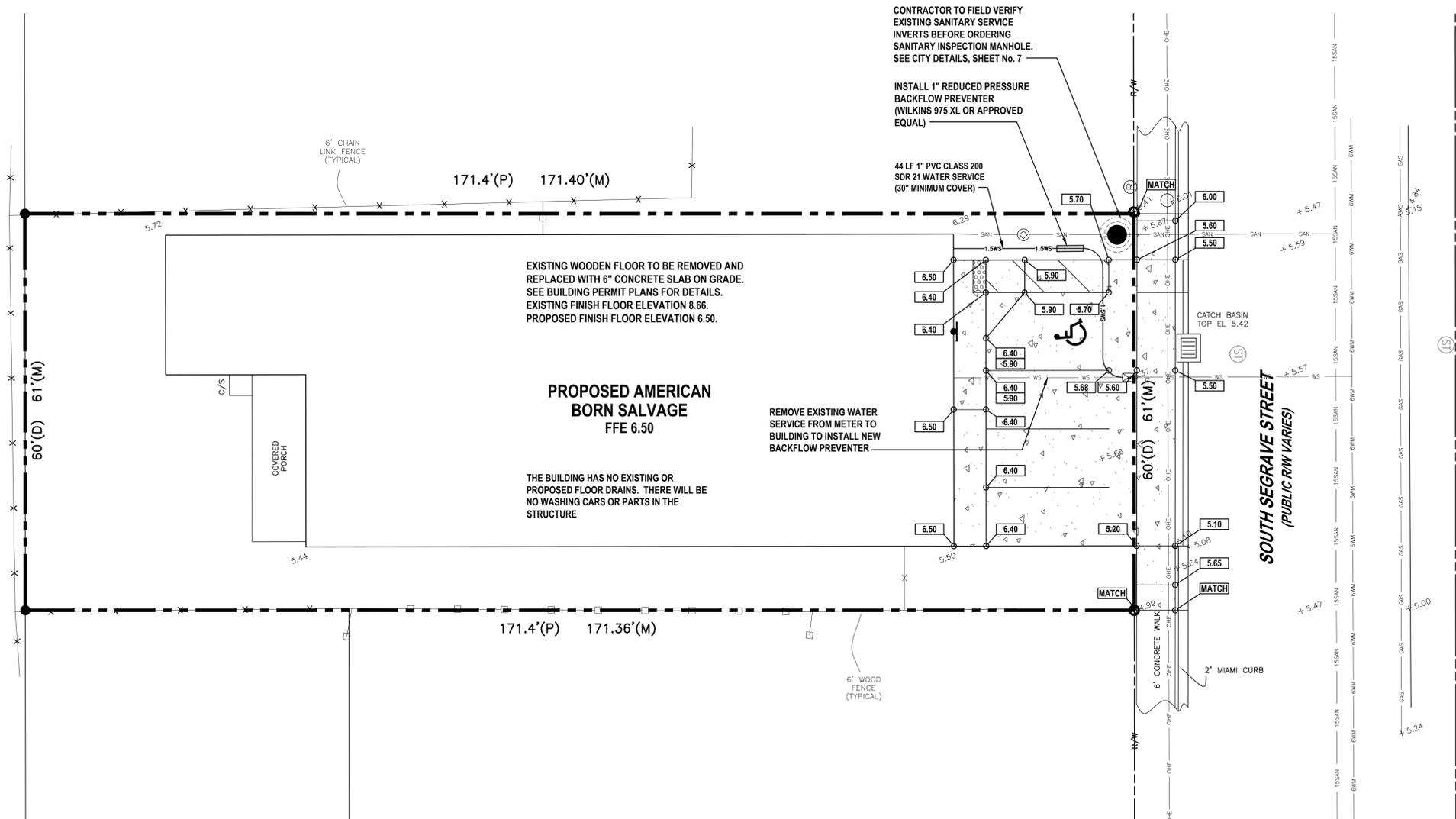
1370 North US1, Suite 204
Ormond Beach, Florida 32174
Phone (386) 290-7599
Harry@Newkirk-Engineering.com

Certificate of Authorization
No. 30209
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Civil Engineering
Land Development
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GRADING, DRAINAGE AND UTILITY PLAN
AMERICAN BORN SALVAGE
142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114



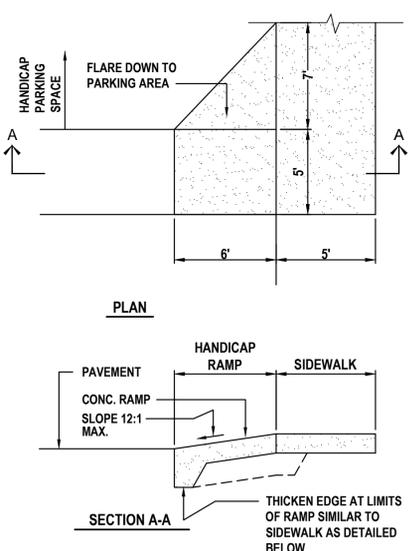
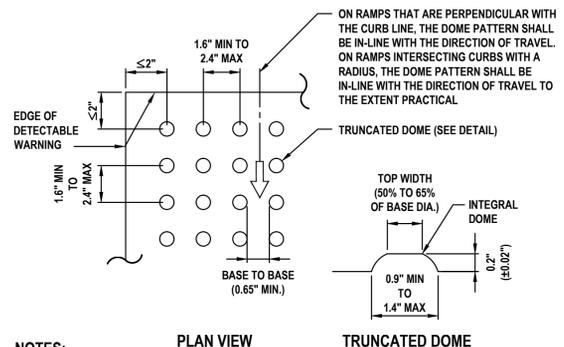
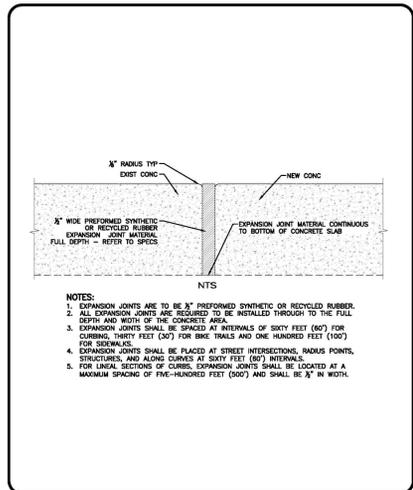
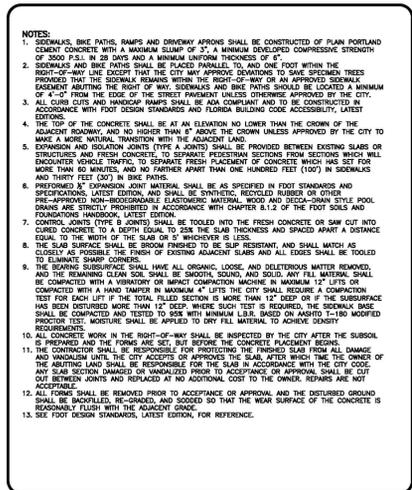
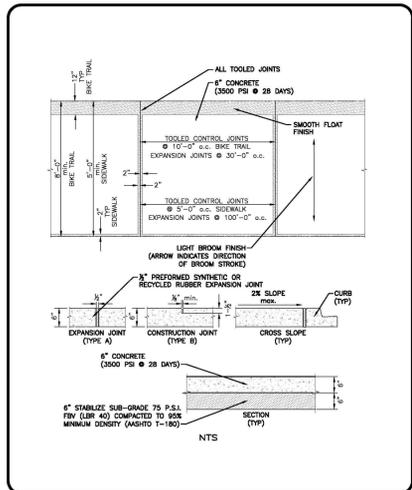
NOT VALID UNLESS SIGNED AND SEALED

PROJECT No:	2015-21
DATE:	FEBRUARY 2015
DESIGN BY:	DAB
DRAWN BY:	HHN
CHECKED BY:	HHN
SCALE:	1" = 10'
DRAWING NUMBER	

6

CITY APPROVAL STAMP
DEV 2015-024

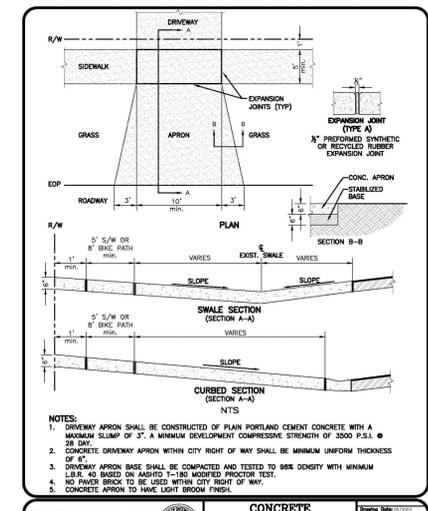
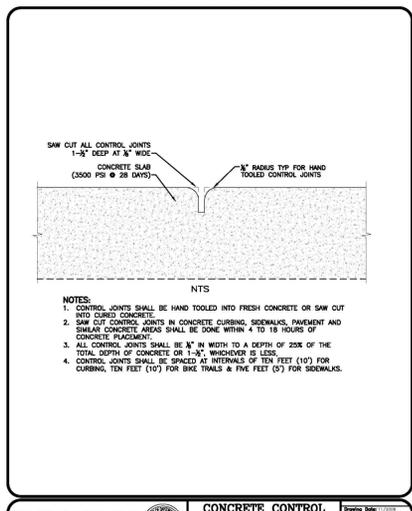
REVISIONS	
DATE	DESCRIPTION
04/01/15	PER CITY COMMENTS



THE CITY OF DAYTONA BEACH
ENGINEERING DIVISION

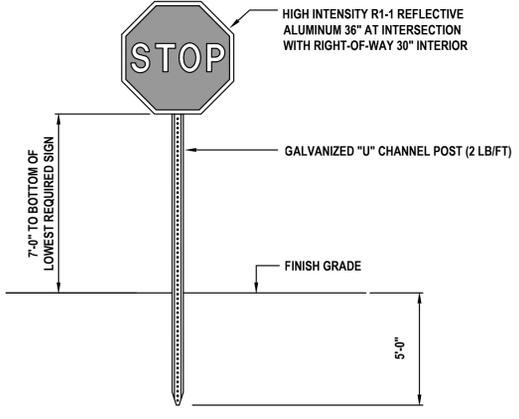
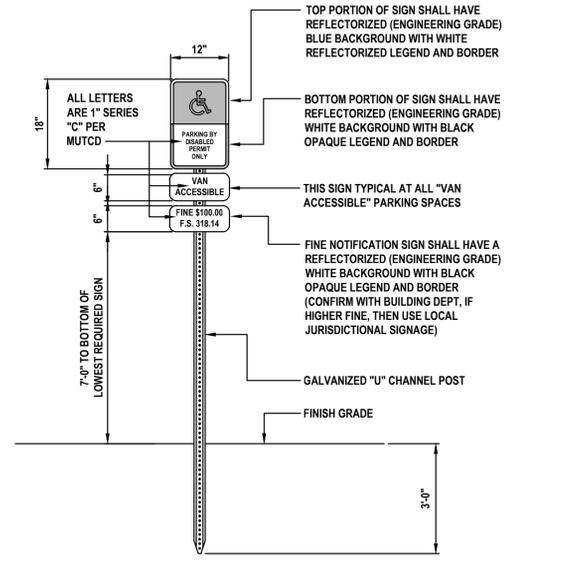
THE CITY OF DAYTONA BEACH
ENGINEERING DIVISION

THE CITY OF DAYTONA BEACH
ENGINEERING DIVISION



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ENGINEERING DIVISION

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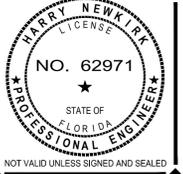
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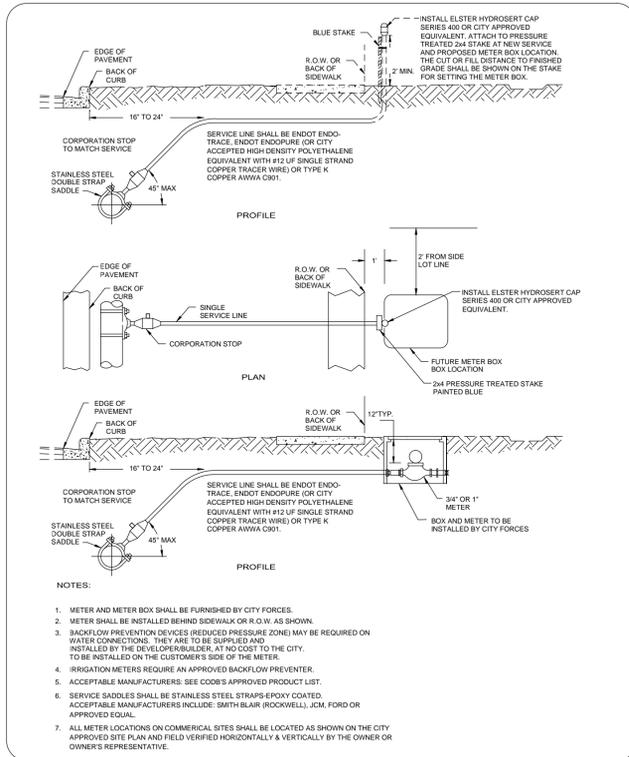
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MISCELLANEOUS DETAILS AND NOTES
AMERICAN BORN SALVAGE
142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114



NOT VALID UNLESS SIGNED AND SEALED

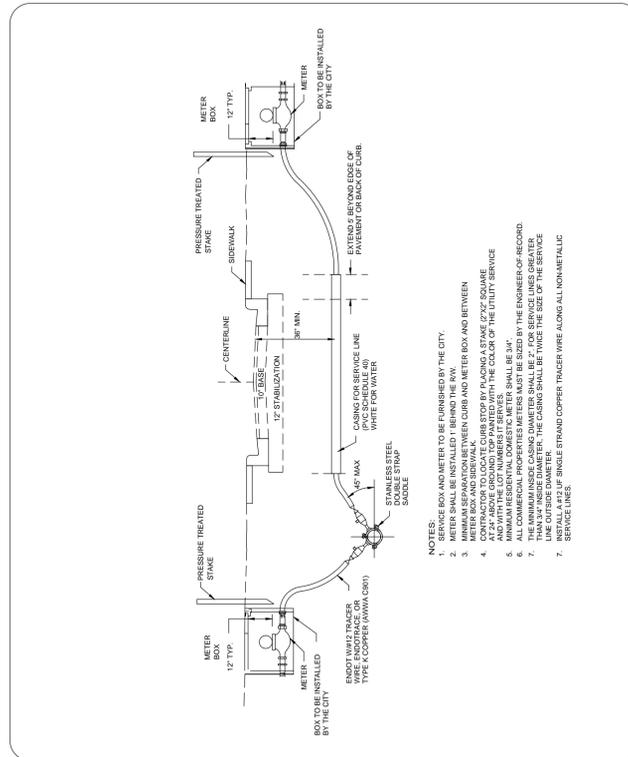
PROJECT No:	2015-21
DATE:	FEBRUARY 2015
DESIGN BY:	DAB
DRAWN BY:	HHN
CHECKED BY:	HHN
SCALE:	AS SHOWN
DRAWING NUMBER	7



THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

WATER SERVICE CONNECTION
DETAIL
W-5A

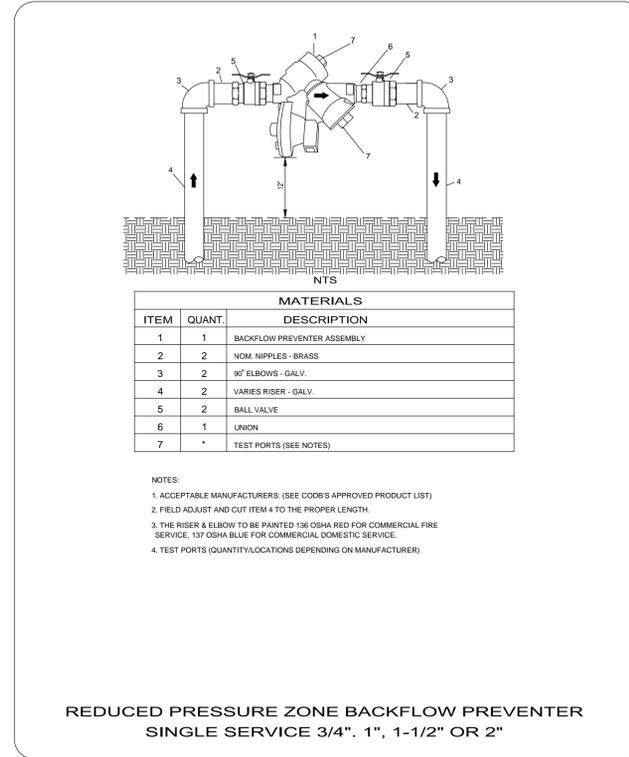
FY-0910
Drawing Date: 01/08
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 07/10
File Name: W-5A



THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

WATER SERVICE CONNECTION (UNDER ROAD)
DETAIL
W-5B

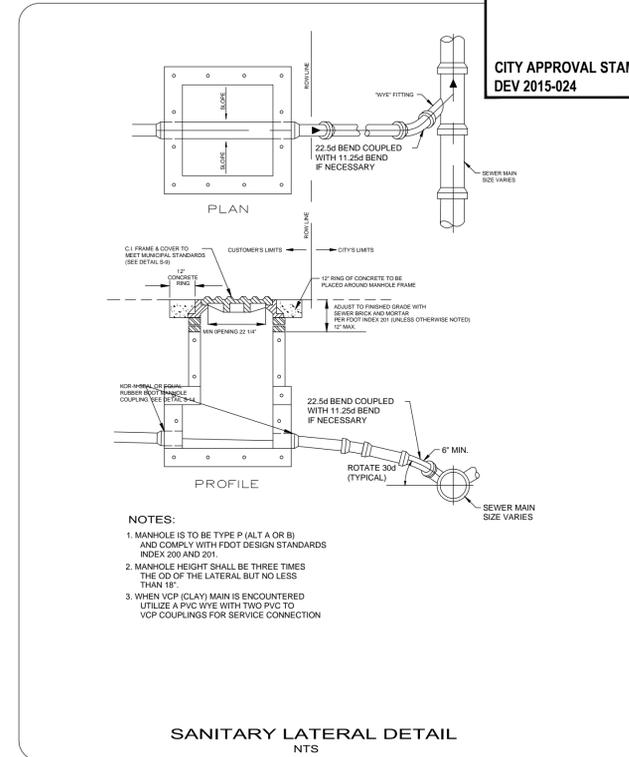
FY-0910
Drawing Date: 01/08
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 07/10
File Name: W-5B



THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

REDUCED PRESSURE ZONE BACKFLOW PREVENTER, SINGLE SERVICE
3/4", 1", 1-1/2" OR 2"
W-17

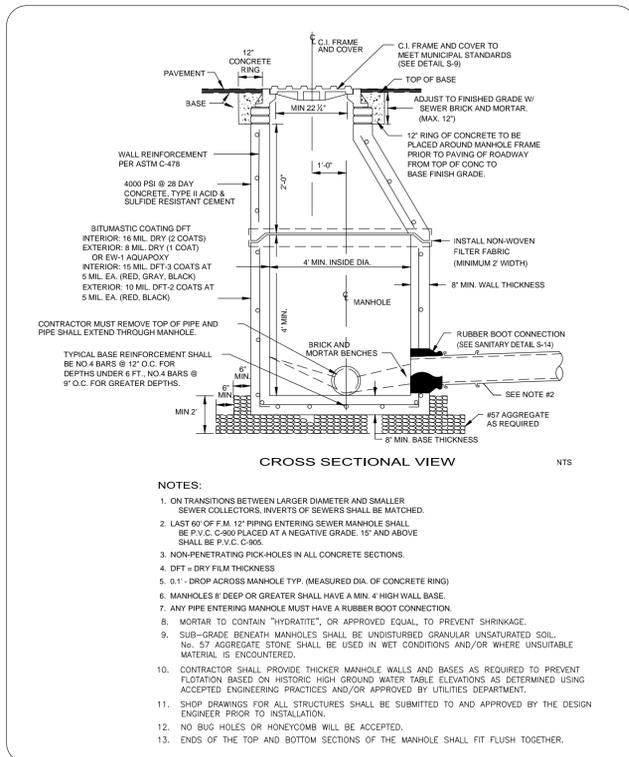
FY-0910
Drawing Date: 01/08
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 07/10
File Name: W-17



THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

COMMERCIAL SANITARY LATERAL DETAIL
S-3B

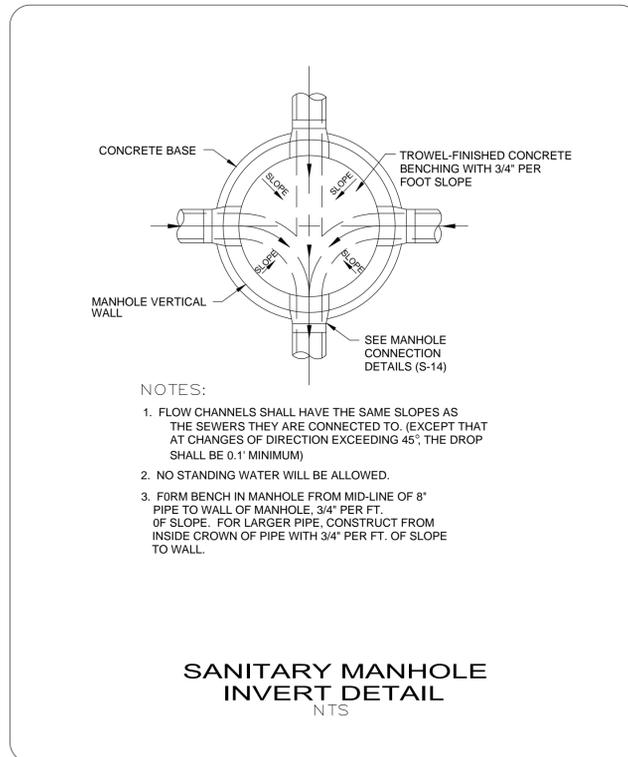
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Drawing Date: 11/10
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 03/08
File Name: S-3B



THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

SANITARY SEWER MANHOLE AND GENERAL NOTES
S-10

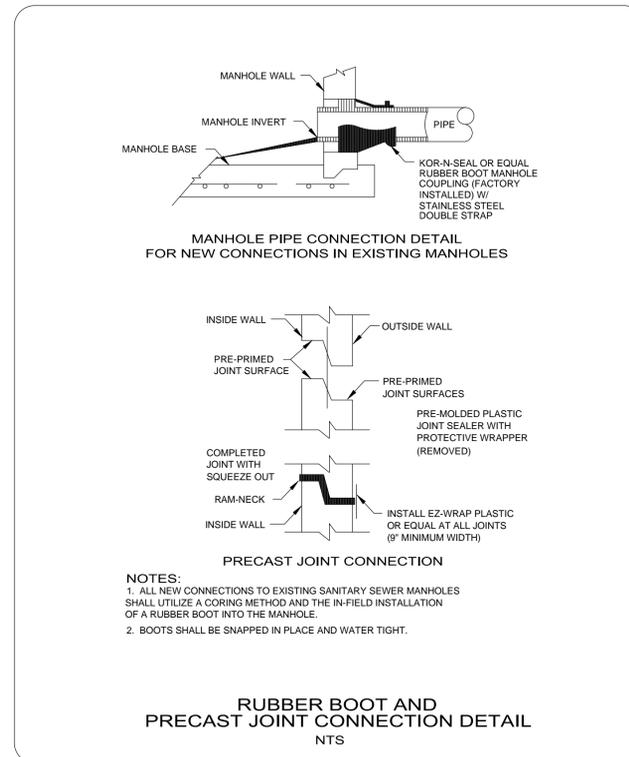
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Drawing Date: 01/08
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 06/10
File Name: S-10



THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

SANITARY MANHOLE INVERT DETAIL
S-13

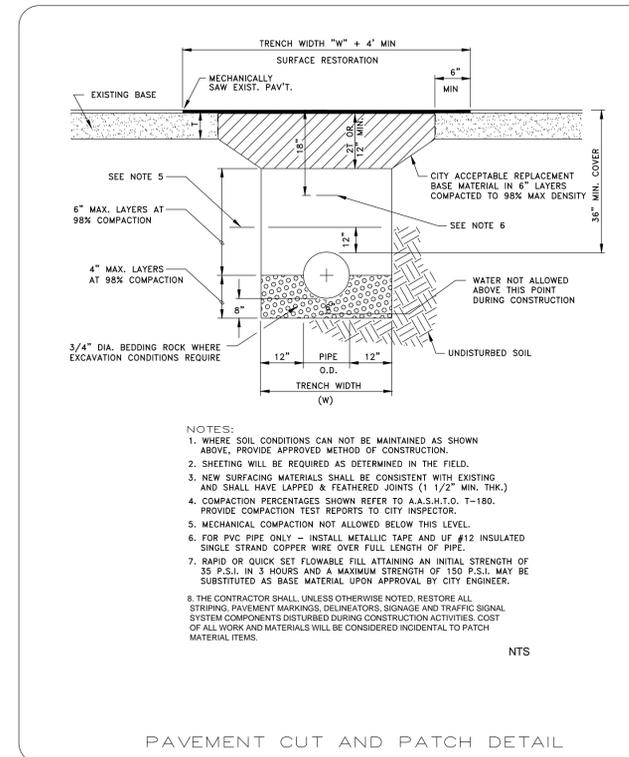
FY-0910
Drawing Date: 01/08
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 06/10
File Name: S-13



THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

RUBBER BOOT AND PRECAST JOINT CONNECTION DETAIL
S-14

FY-0610
Drawing Date: 01/08
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 05/14
File Name: S-14



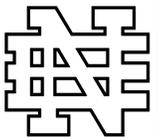
THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT

PAVEMENT CUT AND PATCH DETAIL
S-15

FY-0610
Drawing Date: 01/08
Drawn By: KJH
Checked By: JWP
Scale: NTS
Revision Date: 07/10
File Name: S-15

REVISIONS

DATE	DESCRIPTION
04/01/15	PER CITY COMMENTS



NEWKIRK ENGINEERING

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CITY OF DAYTONA BEACH
DETAILS AND NOTES
AMERICAN BORN SALVAGE
142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114



PROJECT No: 2015-21
DATE: FEBRUARY 2015
DESIGN BY: DAB
DRAWN BY: HHN
CHECKED BY: HHN
SCALE: AS SHOWN
DRAWING NUMBER

8

AS-BUILTS/RECORD DOCUMENTS

PART 1 - GENERAL

1.1 SCOPE OF WORK

- A. CONTRACTOR shall maintain record documents including as-built/record drawings as required in this section.
- B. CONTRACTOR at his expense shall secure the services of a Florida licensed surveyor to collect data and maintain as-built/record drawings according to City of Daytona Beach Utilities Department standards that follows:

As-Built/Record Drawing Requirements

In order to ensure that the City's project records are maintained to the highest standards and the information can easily be added to the City's electronic records the following information is required on all as-built/record drawings.

The intent of these details for as-built/record drawings is for all public facilities constructed by this project. At the pre-construction conference these as-built/record standards will be reviewed to be certain the Contractor's surveyor has a clear understanding of what is required for completion of this work.

1. Pavement and curb widths shall be verified and dimensioned for each street at each block (for subdivisions) and as appropriate to confirm paving limits (on site plans).
2. All radii at intersections shall be verified and dimensioned. This information is to be clearly indicated on the as-built/record drawings.
3. Roadway elevations shall be recorded at all grade changes, 100' intervals along roadway, and other intervals as needed along all streets. Street centerline and curb invert elevations shall be recorded as noted. The as-built centerline profile of all streets shall also be shown on the plan and profile so it may be compared to the design profile grade lines. In the event that the as-built centerline longitudinal grade does not meet the City minimum standards, additional longitudinal grades of the adjacent curbing and similar roadway cross-section surveys to verify the correct cross slope, shall be required to verify that the system will function as originally designed.
4. Storm drainage structures shall be located and/or dimensioned from centerlines or lot lines as appropriate. Each structure shall be located by sub-meter GPS with latitude, longitude and elevation data.

THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT



AS-BUILT DRAWING
REQUIREMENTS
(SHEET 1 OF 6)

FY: 0810
Drawing Date: 01/08
Drawn By: KJA
Checked By: JAP
Scale: NTS
Revision Date: 07/10
File Name: AS-BUILT

AS-BUILTS/RECORD DOCUMENTS
(CONT'D)

5. Storm drainage pipe invert and inlet elevations shall be recorded and clearly denoted as as-built information. Design elevations shall be crossed out and as-built information written next to it.
6. Storm drainage pipe material, length, and size shall be measured and/or verified. This information is to be clearly indicated as being as-built information.
7. All applicable topographic information pertinent to the on-site drainage system, such as ditches, swales, lakes, canals, etc. that are deemed necessary by the City to verify the functional performance of the storm water system, shall be noted. Normally, recording elevations every 100 feet at the top of bank and toe of slope will be required. Measurements shall be taken and recorded in order to accurately tie down these features to the roadway centerlines and to plat lines. Whenever possible, contour lines shall be utilized to graphically describe these topographic features.
8. Retention areas shall have their top of bank and bottom elevations recorded. Actual measurements shall be taken and dimensions recorded of the size of all retention areas. Measurements shall be done from top-of-bank with side slopes indicated. Separate calculations shall be submitted to indicate required and provided retention volumes.
9. Actual materials used and elevations and dimensions of overflow weir structures and skimmers shall be noted on the as-built.
10. Storm drainage swale centerlines shall be located and elevations of flow line and top-of-bank shall be recorded every 100 feet. Side slopes shall also be indicated.
11. Sanitary sewer manholes shall be verified and dimensioned from street centerlines or lot lines as appropriate. All rim and invert elevations shall be verified and recorded. This information shall be clearly indicated as being as-built information. Design elevations shall be crossed out and as-built information written next to it.
12. For subdivisions, proposed design finish floor elevations shall appear on all subdivision lots on the appropriate plan and profile sheet as well as on the master drainage plan.

THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT



AS-BUILT DRAWING
REQUIREMENTS
(SHEET 2 OF 6)

FY: 0809
Drawing Date: 01/08
Drawn By: KJA
Checked By: JAP
Scale: NTS
Revision Date: 07/10
File Name: AS-BUILT

AS-BUILTS/RECORD DOCUMENTS
(CONT'D)

13. Sanitary sewer line lengths, sizes, material, slope, etc., shall be verified and recorded. This information is to be clearly indicated as being as-built information.
14. Sewer laterals shall be verified and recorded at their clean out locations. Stationing and offset distances shall be measured from downstream manholes towards upstream manholes. Invert information at clean out shall be provided, and be located by sub-meter GPS with latitude, longitude and elevation data provided.
15. Lift stations and force mains shall be verified and dimensioned from street centerlines or lot lines as appropriate. Force main depth and location including valves will be provided and tied to permanent above grade features. Dimensional and elevation information indicated on the approved plan shall be verified and recorded. This information shall be clearly indicated as being as-built information. Buried potable water line and electrical service line shall be clearly dimensioned, located, and labeled. Each lift station shall be located by sub-meter GPS with latitude, longitude and elevation data provided.
16. Curb cuts or metal tabs used to mark sewer laterals, water services and water valves shall be verified for presence and accuracy of location.
17. Potable and reclaimed water main lines shall be dimensioned off the baseline construction. Water main line material size, length and depth placed shall be noted. Locations of valves shall also be tied to baseline construction. This information shall be clearly indicated as being as-built information.
18. Potable and reclaimed water valves, tees, bends, all services, and fire hydrants shall be located by tying them to baseline construction (Sta. & Offset). Similarly, force main valves, tees and bends shall be located in the same manner. Stationing and offset distances shall be measured from downstream manholes to upstream manholes. All valves and hydrants shall be located by sub-meter GPS with latitude, longitude and elevation data provided.
19. For perpendicular crossings of storm water, sanitary sewer, potable water, or reclaimed water, the as-built plans shall clearly indicate which utilities are located over or under other utilities, as necessary.

THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT



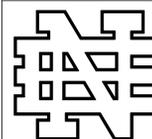
AS-BUILT DRAWING
REQUIREMENTS
(SHEET 3 OF 6)

FY: 0809
Drawing Date: 01/08
Drawn By: KJA
Checked By: JAP
Scale: NTS
Revision Date: 07/10
File Name: AS-BUILT

REVISIONS

DATE	DESCRIPTION
04/01/15	PER CITY COMMENTS

CITY APPROVAL STAMP
DEV 2015-024



**NEWKIRK
ENGINEERING**

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CITY OF DAYTONA BEACH
AS-BUILT REQUIREMENTS
AMERICAN BORN SALVAGE
142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114



NOT VALID UNLESS SIGNED AND SEALED

PROJECT No: 2015-21

DATE: FEBRUARY 2015

DESIGN BY: DAB

DRAWN BY: HHN

CHECKED BY: HHN

SCALE: AS SHOWN

DRAWING NUMBER

9

AS-BUILTS/RECORD DOCUMENTS
(CONT'D)

20. Any special features such as concrete flumes, lake banks, walls, fencing, etc. which were a part of the approved construction drawings should also be located and dimensioned.
21. If an approved subdivision plat or site plan shows a conservation easement, the project surveyor should provide the exact location of the specimen tree(s) from the right-of-way or property lines and proposed easement boundaries on the as-built drawing. The as-built location of these trees will help verify the sufficiency of the conservation easement prior to plat recording or certificate of occupancy.
22. When storm water, potable water, reclaimed water, or sanitary sewer improvements are located within an easement, the as-built drawing shall accurately depict the location of the easement itself as well as the exact location of the improvements within the easement. This is required in order to verify that the improvements have been properly located and to ensure that future subsurface excavation to perform remedial repair can be accomplished without disturbance beyond the easement.
23. As-built drawings are to be prepared by a Florida licensed surveyor and shall include a signed certification statement by the Florida licensed engineer of record. A Mylar set of as-built record drawings shall be provided with a digital copy in a compatible AutoCad format.
24. Elevations shall be referenced to NGVD 1988 Data. As-built survey information shall be referenced to at least two Florida State Plane east coordinates NAD 83.
25. Benchmark Datum utilizes North American Vertical Datum of 1929 monumentation with elevations adjusted to NGVD 1988 data. Any NAVD 1929 monument within the limits of construction is to be protected.

1.2 SUBMITTALS

- A. CONTRACTOR shall submit each month to CITY the draft RECORD DOCUMENTS that show current construction activities AND a copy of notices to agencies including the City regarding road closures; plus a RECORD OF EVENTS that may be needed in the future.
- B. CONTRACTOR shall submit to CITY as required the proposed shut-off schedule, capping, temporary service scheduling, record of notices to customers and proposed roadway closings.

THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT



AS-BUILT DRAWING
REQUIREMENTS
(SHEET 4 OF 6)

FY: 0809
Drawing Date: 01/08
Drawn By: KJA
Checked By: JAP
Scale: NTS
Revision Date: 07/10
File Name: AS-BUILT

AS-BUILTS/RECORD DOCUMENTS
(CONT'D)

- C. CONTRACTOR shall submit when requested copies of published notices.
- D. CONTRACTOR shall submit Record Drawings on CD and Mylar. When the As-Built are delivered for clearance of water lines (two paper copies), they will be scheduled for chlorination. CITY will not release the drinking water bacteriological laboratory report to the Volusia County Health Department until the As-built information meets CITY requirements. CONTRACTOR will have 60 days from the time the bacteriological samples are collected to submit the as-built Mylar and CD to the CITY. Send the two paper copies for approval before making the Mylar. If CONTRACTOR goes past the 60 days, re-chlorination will be required and pay for the bacteriological laboratory report will be required. Below are minimum detail samples of how the As-built drawing information will need to be presented:

Example of how we need to see the valves, fittings, and pipes labeled, with an arrow going to the object.

Valve Example:

20" GATE VALVE
STA. 22+33.54 LT. 55.32'
LAT=29°12'53.009"N
LONG=81°04'03.355"W
TOP OF VALVE ELEV.=27.50
FINISH GROUND ELEV.=30.50

Benchmark Example:

BM#13
STA. 20+33.65 LT. 55.83'
N=1,774,373.4058
E=634,602.7566
LAT=29°04'53.355"N
LONG=81°04'53.355"W
ELEV.=32.55

(A Bench Mark must be shown on the site plans)
ALL DRAWINGS TO BE DRAWN TO TRUE STATE PLANE COORDINATES

Pipes Example:

20" WATER MAIN
STA. 22+00.43 LT. 55.24'
LAT=29°12'50.009"N
LONG=81°04'26.355"W
TOP OF PIPE ELEV.=23.43

THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT



AS-BUILT DRAWING
REQUIREMENTS
(SHEET 5 OF 6)

FY: 0809
Drawing Date: 01/08
Drawn By: KJA
Checked By: JAP
Scale: NTS
Revision Date: 07/10
File Name: AS-BUILT

AS-BUILTS/RECORD DOCUMENTS
(CONT'D)

PART 2 - EXECUTION

2.1 GENERAL

All materials, equipment, and labor needed to ensure accurate AS BUILT/RECORD DRAWINGS shall be provided by the CONTRACTOR.

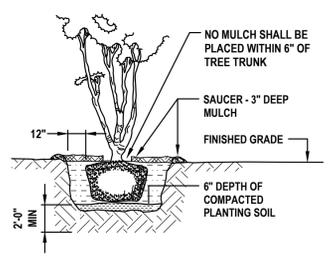
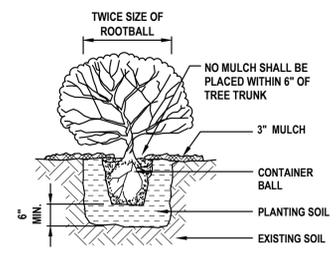
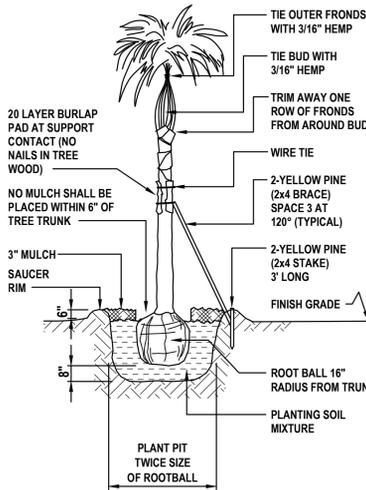
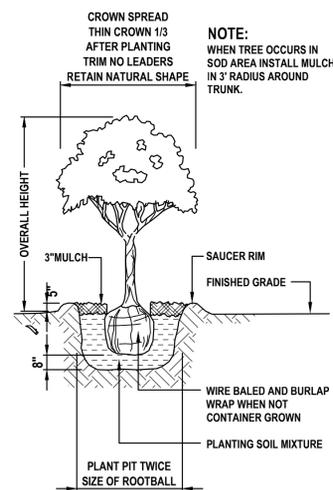
- A. It is acceptable to the CITY if the surveyor utilizes an after the fact approach to collecting and verifying the location and depth by vertical PVC pipes placed by the CONTRACTOR as markers for this purpose. The surveyor shall verify to accuracy defined in Florida Statutes the As-built conditions and certify the RECORD DRAWINGS.
- B. The CITY'S REPRESENTATIVE shall not be considered the best source of information for valve locations that may have been lost during final grading. The surveyor or CONTRACTOR shall excavate and properly mark all valve boxes and each valve shall have a tag or be color coded to define water, sewer or reuse water valves. The use of temporary PVC pipe markers color coded is acceptable so long as cross references are provided on the RECORD DRAWINGS to prevent the tops from a water valve being placed on a sewer valve.
- C. THE CONTRACTOR SHALL PROVIDE THE UTILITIES DEPARTMENT ENGINEERING DIVISION THE FINAL AS-BUILT/RECORD DRAWINGS ON CD AND MYLARS. THE AS-BUILT/RECORD DRAWINGS SHALL BE PREPARED AND PRESENTED IN MODEL SPACE USING AUTOCAD FORMAT 2007 OR LATER. THE MODEL SPACE DRAWING SHALL BE IN FL83-EF STATE PLANE AND SHALL BE ABLE TO BE INSERTED INTO THE CITY'S OVERALL GIS SYSTEM. THE RECORD DRAWINGS SHALL ALSO BE PRINTED ON MYLAR SIGNED AND SEALED AS ALLOWED BY STATE OF FLORIDA REGULATIONS. A DISCLAIMER MAY BE NOTED IN A TRANSMITTAL LETTER PLUS THE SURVEYOR MAY ADD A SPECIAL NOTICE ON EACH SHEET REGARDING THE LOCATION OF THE TRUE ORIGINAL RECORD DRAWINGS OR PLACE LIMITS ON RESPONSIBILITY SHOULD IN THE FUTURE SOMEONE MODIFY THE MYLARS.
- D. Identify the source markers for the survey used for Record Drawings.

THE CITY OF DAYTONA BEACH
UTILITIES DEPARTMENT



AS-BUILT DRAWING
REQUIREMENTS
(SHEET 6 OF 6)

FY: 0809
Drawing Date: 01/08
Drawn By: KJA
Checked By: JAP
Scale: NTS
Revision Date: 07/10
File Name: AS-BUILT



LANDSCAPE NOTES:

- ALL PLANT MATERIAL SHALL CONFORM TO THE STANDARDS FOR FLORIDA #1 OR BETTER AS DESCRIBED IN THE CURRENT "GRADES AND STANDARDS FOR NURSERY PLANTS", STATE OF FLORIDA, DEPARTMENT OF AGRICULTURE, TALLAHASSEE, OR THEIR EQUAL AS DETERMINED BY THE LANDSCAPE ARCHITECT.
- ALL MULCH SHALL BE ALL NATURAL WOOD (NO CYPRESS).
- SOD SPECIFIED AS ST. AUGUSTINE "FLORITAM" SHALL BE 95% WEED-FREE. SOD SPECIFIED AS BAHIA SHALL BE 95% WEED-FREE. ALL SOD SHALL BE INSTALLED WITH TIGHT JOINTS, ROLLED AND FERTILIZED. SEED, IF ANY, TO MEET SOUTHERN SEED CERTIFICATION ASSOCIATION.
- ALL DIMENSIONS SHALL BE FIELD-CHECKED BY THE LANDSCAPE CONTRACTOR PRIOR TO CONSTRUCTION, WITH ANY DISCREPANCIES REPORTED IMMEDIATELY TO THE LANDSCAPE ARCHITECT.
- THE PLANT MATERIALS SCHEDULE IS PROVIDED FOR THE CONVENIENCE OF THE LANDSCAPE CONTRACTOR; SHOULD THERE BE ANY DISCREPANCY BETWEEN THE PLAN AND THE PLANT LIST, THE PLAN WILL PREVAIL.
- ALL MATERIALS MUST BE AS SPECIFIED ON THE LANDSCAPE PLAN. IF MATERIALS OR LABOR DO NOT ADHERE TO THE SPECIFICATIONS, THEY WILL BE REJECTED AT NO ADDITIONAL COST TO THE OWNER.
- ALL PLANTING BEDS SHALL BE TREATED WITH "ROUND-UP" AND "RONSTAR". USE AS DIRECTED BY MANUFACTURERS.
- NO SUBSTITUTIONS OR CHANGES OF ANY KIND WILL BE ALLOWED AT THE TIME OF BIDDING SO AS TO PROVIDE FOR FAIR COMPARISON.
- EXISTING IRRIGATION SYSTEM SHALL BE REVAMPED TO PROVIDE 100% COVERAGE.
- THE CONTRACTOR SHALL VERIFY EXISTENCE AND LOCATION OF ALL EXISTING UTILITIES AND UNDERGROUND CONDITIONS PRIOR TO HIS COMMENCEMENT OF THE ANY WORK.
- ALL BUILDING MATERIALS AND LABOR SHALL CONFORM TO THE SOUTHERN BUILDING CODE AND ALSO TO ALL LOCAL CODES THAT HAVE JURISDICTION.
- ALL PERMIT AND VARIANCE APPLICATIONS SHALL BE MADE BY THE CONTRACTOR.
- PRIOR TO CONSTRUCTION OF PLANTING BEDS, ALL AREAS ARE TO HAVE SOIL TESTS CONDUCTED TO DETERMINE pH AND SOIL FERTILITY. IT IS THE RESPONSIBILITY OF THE LANDSCAPE CONTRACTOR TO AMEND THE SOIL TO MEET ADEQUATE FERTILITY AND pH FOR CORRESPONDING PLANT MATERIAL. ALL TEST RESULTS SHALL BE REPORTED TO THE LANDSCAPE ARCHITECT.
- EQUIPMENT SHALL BE OPERATED IN A MANNER AS NOT TO INJURE OR DESTROY ANY TREES SHOWN TO REMAIN. CONTRACTOR SHALL NOT CAUSE OR ALLOW THE CLEANING OF EQUIPMENT OR MATERIAL WITHIN THE DRIP LINE OF ANY TREE OR GROUPS OF TREES TO BE RETAINED OR THOSE PROPOSED. NOR SHALL THE CONTRACTOR ALLOW THE DISPOSAL OF WASTE MATERIAL, SUCH AS PAINT, OIL SOLVENTS, ASPHALT, CONCRETE, MORTAR OR ANY OTHER MATERIAL HARMFUL TO THE LIFE OF A TREE WITHIN THE DRIP LINE OF ANY TREE OR GROUP OF TREES. NO ATTACHMENT, WIRES (OTHER THAN PROTECTIVE GUY WIRES), SIGNS, OR PERMITS MAY BE FASTENED TO A TREE.
- ANY EXISTING TREES CREDITED TOWARDS REQUIRED BUFFERS OR LANDSCAPE REQUIREMENTS REMOVED FOR ANY REASON SHALL BE REPLACED WITH TREES MEETING CITY APPROVAL WITH REGARDS TO SPECIES AND SIZE.
- ALL QUESTIONS CONCERNING THE PLAN AND/OR SPECIFICATIONS SHALL BE DIRECTED TO THE LANDSCAPE ARCHITECT (386) 672-9515.
- ALL SHADE TREES AND SINGLE TRUNK UNDERSTORY TREES SHALL BE STAKED USING ARBORGYU STAKING SYSTEM FOR SINGLE STEM TREE AND ALL MULTI-TRUNK UNDERSTORY TREES SHALL BE STAKED WITH ARBORGYU MULTI-TRUNK SYSTEM.
- ANY CHANGE IN STAKING SYSTEM MUST BE APPROVED IN WRITING BY THE LANDSCAPE ARCHITECT BEFORE BEING INSTALLED.
- ALL UNIMPROVED AREA NOT OTHERWISE PLANTED OR MULCHED SHALL BE SODDED WITH ST. AUGUSTINE FLORITAM UNLESS OTHERWISE NOTED ON THE LANDSCAPE PLAN.
- PLANT MATERIAL SHALL BE CLEARLY IDENTIFIED AS FLORIDA #1 OR BETTER ON EITHER LABELS OR INVOICES.
- ALL PLANT MATERIAL SHALL BE GUARANTEED ONE YEAR AFTER ACCEPTANCE BY OWNER.
- ALL TREES IN SOD TO BE IN A (4) FOOT MINIMUM-MULCHED RING AROUND.
- TREES SHALL BE PLANTED SO THAT THE TRUNK FLARE IS EXPOSED AND TOPMOST ROOT IN THE ROOTBALL ORIGINATING FROM THE TRUNK IS AT SOIL SURFACE OR WITHIN THE TOP INCH OF SOIL ON THE ROOTBALL.
- ALL PLANT SPECIFICATIONS MUST BE MET OR EXCEEDED.
- PLANT TREE SO THAT ROOTBALL IS 1"-2" ABOVE FINISH GRADE.
- REMOVAL OF ALL CONSTRUCTION DEBRIS, LIMEROCK, EXCESS OF BUILDERS SAND, CONCRETE AND MORTAR DEBRIS, EXISTING WEEDS AND GRASS, AND ALL FOREIGN MATERIALS IN THE PLANTING BED AND SOD AREAS SHALL BE REMOVED AND A MINIMUM OF 3" OF CLEAN SAND WITH A pH 5.5-6.5 SHALL BE INSTALLED PRIOR TO ANY INSTALLATION OF PLANTS OR TREES.
- FOR ALL NEW DEVELOPMENT, OR REDEVELOPMENT OF EXISTING PROPERTY, THE APPLICANT SHALL BE REQUIRED TO REMOVE ALL INVASIVE NONNATIVE PLANT SPECIES FROM THE PROPERTY PRIOR TO ISSUANCE OF THE CERTIFICATE OF OCCUPANCY.
- ALL EXISTING TREES AND PALMS WILL BE PROPERLY PRUNED AND CLEANED OF DEADWOOD, BROKEN BRANCHES, DEAD FROND AND VINES AS NEEDED.

PLANTING SCHEDULE

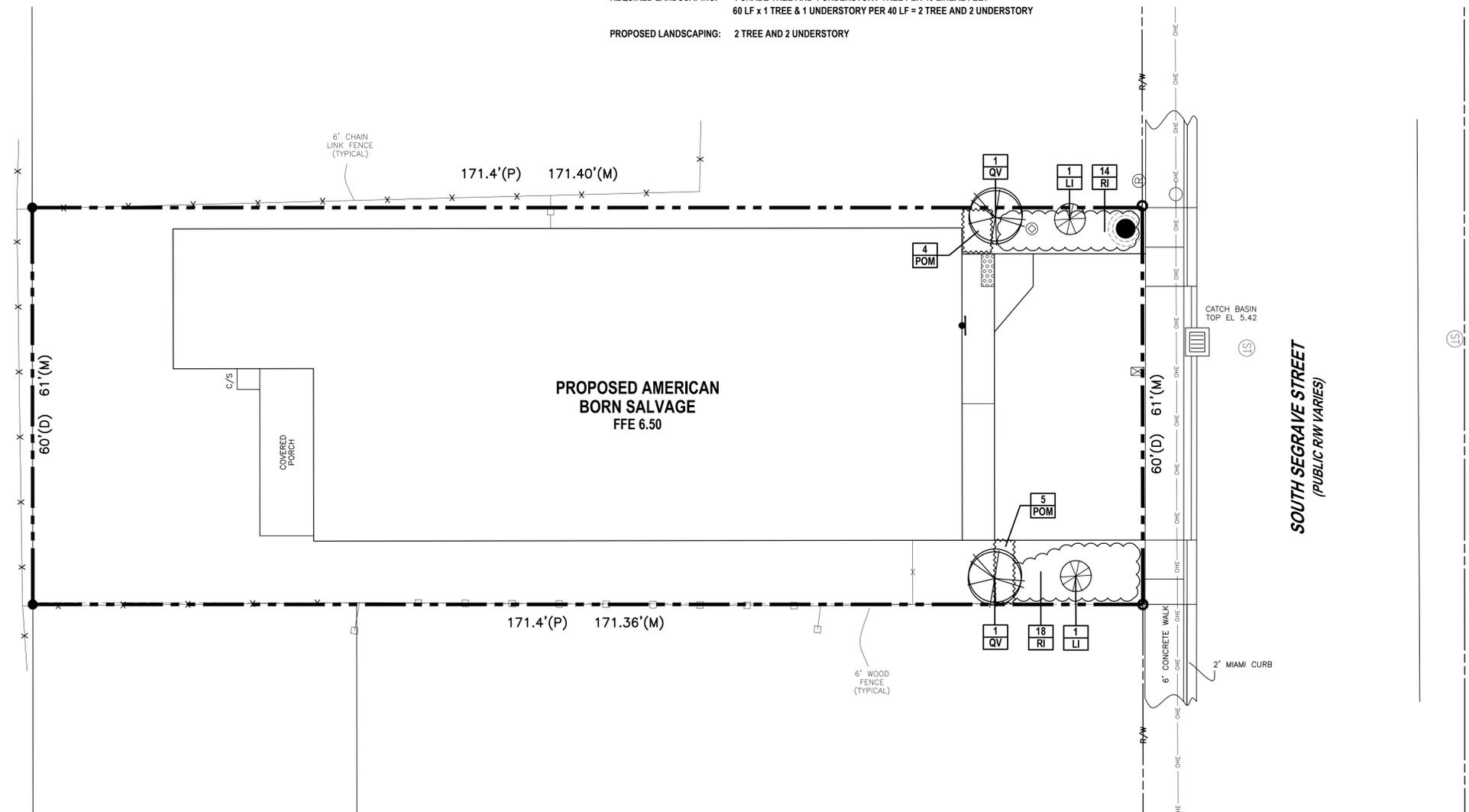
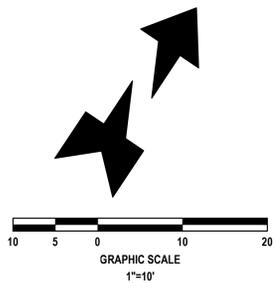
QUANTITY	CODE	BOTANICAL NAME	COMMON NAME	SPECIFICATIONS
2	LI	LAGERSTROEMIA INDICA	CRAPE MYRTLE SSP - PURPLE (STANDARD)	8' HGT., FULL, 1.5" CAL. @ DBH
9	POM	PODOCARPUS MACROPHYLLUS	YEW PODOCARPUS	7 GAL., 36" HGT., 30" O.C.
32	RI	RAPHIOLEPIS INDICA	INDIAN HAWTHORNE	3 GAL., 16" HGT., FULL
2	QV	QUERCUS VIRGINIANA	LIVE OAK	10'-12" HGT., FULL, 2.5" CAL. @ DBH

NOTE: ALL TREES (NEW AND EXISTING) ARE TO HAVE A 3" DIAMETER MULCHED CIRCLE AROUND THE BASE UNLESS IT IS IN A PLANTING BED.

NOTE:
ALL LANDSCAPING ALONG SEAGRAVE STREET ARE IN COMPLIANCE WITH THE CITY OF DAYTONA BEACH LAND DEVELOPMENT CODE, ARTICLE 6.4.

REQUIRED SEAGRAVE BUFFER LANDSCAPING

- REQUIRED LANDSCAPING: 1 SHADE TREE AND 1 UNDERSTORY TREE PER 40 LINEAL FEET
60 LF x 1 TREE & 1 UNDERSTORY PER 40 LF = 2 TREE AND 2 UNDERSTORY
- PROPOSED LANDSCAPING: 2 TREE AND 2 UNDERSTORY



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RLP

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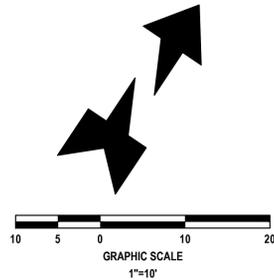
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AMERICAN BORN SALVAGE
142 SOUTH SEGRAVE STREET
DAYTONA BEACH, FL 32114



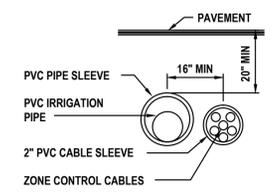
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PIPE SIZING INFORMATION

GALLONS PER MINUTE (GPM)	PIPE SIZE	PIPE SLEEVE SCHEDULE	
		PIPE SIZE	SLEEVE SIZE
0-6	1/2"	2 1/2"	6"
7-10	3/4"	2 1/2"	6"
11-16	1"	1" THRU 2"	4"
17-28	1 1/4"	1/2" AND 3/4"	3"
29-35	1 1/2"		
35-55	2"		

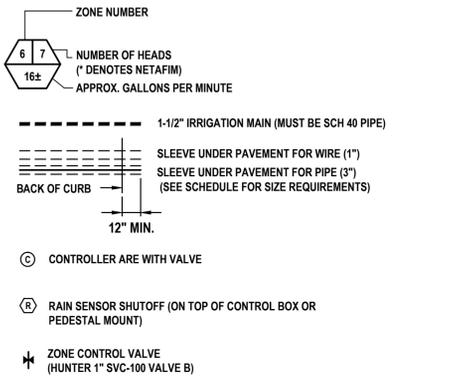


NOTE:

1. THE IRRIGATION CONTRACTOR SHALL BE RESPONSIBLE TO COMPLY WITH THE CITY AND COUNTY WATER WISE ORDINANCE REQUIREMENTS. THE IRRIGATION CONTRACTOR SHALL NOTIFY THE LANDSCAPE ARCHITECT IF THERE ARE ANY DEVIATIONS TO THE DRAWING.
2. IRRIGATION LINES ARE SHOWN DIAGRAMMATICALLY AND ARE INTENDED TO SHOW DISTRIBUTION ZONES ONLY. ALL VALVES SHALL BE LOCATED WITHIN PLANTING AREAS (NOT WITHIN PAVEMENT). LINES LOCATED UNDER PAVEMENT SHALL BE KEPT TO A MINIMUM AND ALL PIPING UNDER PAVED AREAS SHALL BE SLEEVED.
3. WHEN INSTALLING IRRIGATION PIPING IN ISLAND AND OTHER NARROW PLANTING AREAS RUN PIPING CLOSE TO CURB AND NOT DOWN THE MIDDLE OF THE PLANTING AREA. (BEFORE DOING IRRIGATION GET A COPY OF THE LANDSCAPE PLAN AND KEEP IRRIGATION LINES OUT OF PLANTING AREAS WHERE POSSIBLE.)
4. NO SIGNIFICANT IRRIGATION OVERTHROW SHALL BE ALLOWED ONTO IMPERVIOUS SURFACES.

IRRIGATION LEGEND

15 SERIES MPR	FLOW RATE (GPM)	FLOW RATE (GPM)
Ⓐ ADJUSTABLE 15' RADIUS	VARIABLES	15' EST 0.61
Ⓑ QUARTER	0.92	15' SST 1.21
Ⓒ HALF	1.85	9' SST 1.73
Ⓓ THREE QUARTER	2.78	15' CST 1.21
Ⓔ FULL	3.70	



NOTE: ALL HEADS MUST BE PRESSURE COMPENSATING OR ALL VALVES MUST BE PRESSURE COMPENSATING.

IRRIGATION NOTES:

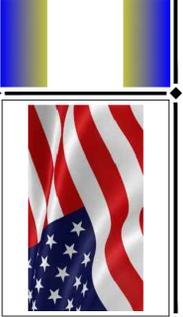
1. THE CONTRACTOR SHALL VERIFY THE EXISTENCE AND LOCATION OF ALL EXISTING UTILITIES AND CONDITIONS PRIOR TO ITS COMMENCEMENT OF THE IRRIGATION WORK.
2. CHECK PRESSURE AND GPM OF WATER SUPPLY BEFORE BEGINNING JOB AND REPORT FINDING TO LANDSCAPE ARCHITECT. LANDSCAPE ARCHITECT WILL MAKE ANY ADJUSTMENT NECESSARY TO MAKE SYSTEM WORK AT ITS BEST.
3. THE PLAN IS SCHEMATIC ONLY. THE CONTRACTOR SHALL INSTALL PIPING IN A MINIMUM NUMBER OF TRENCHES AND SHALL INSTALL PIPE IN A MINIMUM LENGTH.
4. QUANTITIES FOR IRRIGATION MATERIALS ARE NOT GIVEN. THE CONTRACTOR SHALL DETERMINE THIS FROM THE PLAN.
5. INSTALL SLEEVE PIPING WHERE SHOWN ON THE DRAWINGS AT THE PROPER DEPTH.
6. ALL SLEEVE PIPE SHALL BE SCHEDULE 40 PVC PIPE INSTALLED A MINIMUM OF 20" BELOW FINISHED PAVING GRADES.
7. ALL SLEEVES WHEN PLACED IN FIELD ARE TO BE LOCATED BY A METAL PIPE AT EACH END AND LOCATED FROM TWO STATIONARY POINTS BY TAPE MEASUREMENTS.
8. ALL PIPE SHALL BE INSTALLED A MINIMUM OF 20" BELOW GRADE.
9. ALL TRENCHING SHALL BE KEPT OUT OF THE DRIP LINE AREA OF ALL EXISTING TREES. USE RADIAL LINES OR TUNNELING WHEN NECESSARY TO ENCR OACH INTO THE DRIP LINE AREA OF TREES.
10. LOCATE ALL VALVES AND OTHER IRRIGATION EQUIPMENT IN PLANT BED AREAS WITHIN THE PROJECT LIMITS FOR CONCEALMENT PURPOSES.
11. RISERS ARE TO BE HIDDEN COMPLETELY IN SHRUBBERY OR PAINTED BLACK AND IN NO CASE BE HIGHER THAN THE SHRUBBERY INSTALLED.
12. ALL PIPE EXPOSED ABOVE GRADE AND TO VIEW SHALL BE SCHEDULE 40 GALVANIZED STEEL PIPE OF THE NOTED SIZE.
13. ALL IRRIGATION EQUIPMENT (PUMP, CONTROLLER, ETC.) SHALL BE PLACED WITHIN FENCED ENCLOSURE.
14. ALL VALVES SHALL BE INSTALLED IN METER TYPE SIZE BOXES EQUAL TO AMETEK POLY-IRON.
15. PROVIDE A 6" GRAVEL SUMP AT THE BOTTOM OF ALL METER BOXES AND INSTALL 1/2" TO 1" DIAMETER GRAVEL AT THE BOTTOM OF THE VALVE PIT.
16. IRRIGATION SHALL MEET ALL APPLICABLE CURRENT MUNICIPAL, COUNTY, STATE OR FEDERAL CODES, ORDINANCES AND REGULATIONS THAT HAVE JURISDICTION.
17. ALL PIPE 1/2" TO 2 1/2" IN SIZE SHALL BE PRESSURE RATED 160 PVC (EXCEPT MAIN).
18. ALL FITTINGS SHALL BE SCHEDULE 40 PVC.
19. ALL SPRAY HEADS ARE TO BE A MINIMUM OF 6" POP-UP AND ALL HEADS IN PLANTING AREAS TO BE 12" POP-UP OR RISERS. RISERS SHALL BE PAINTED GREEN OR BLACK.
20. ELECTRICAL TO CONTROLLER SHALL BE SUPPLIED BY ELECTRICAL CONTRACTOR (NOT IRRIGATION CONTRACTOR)

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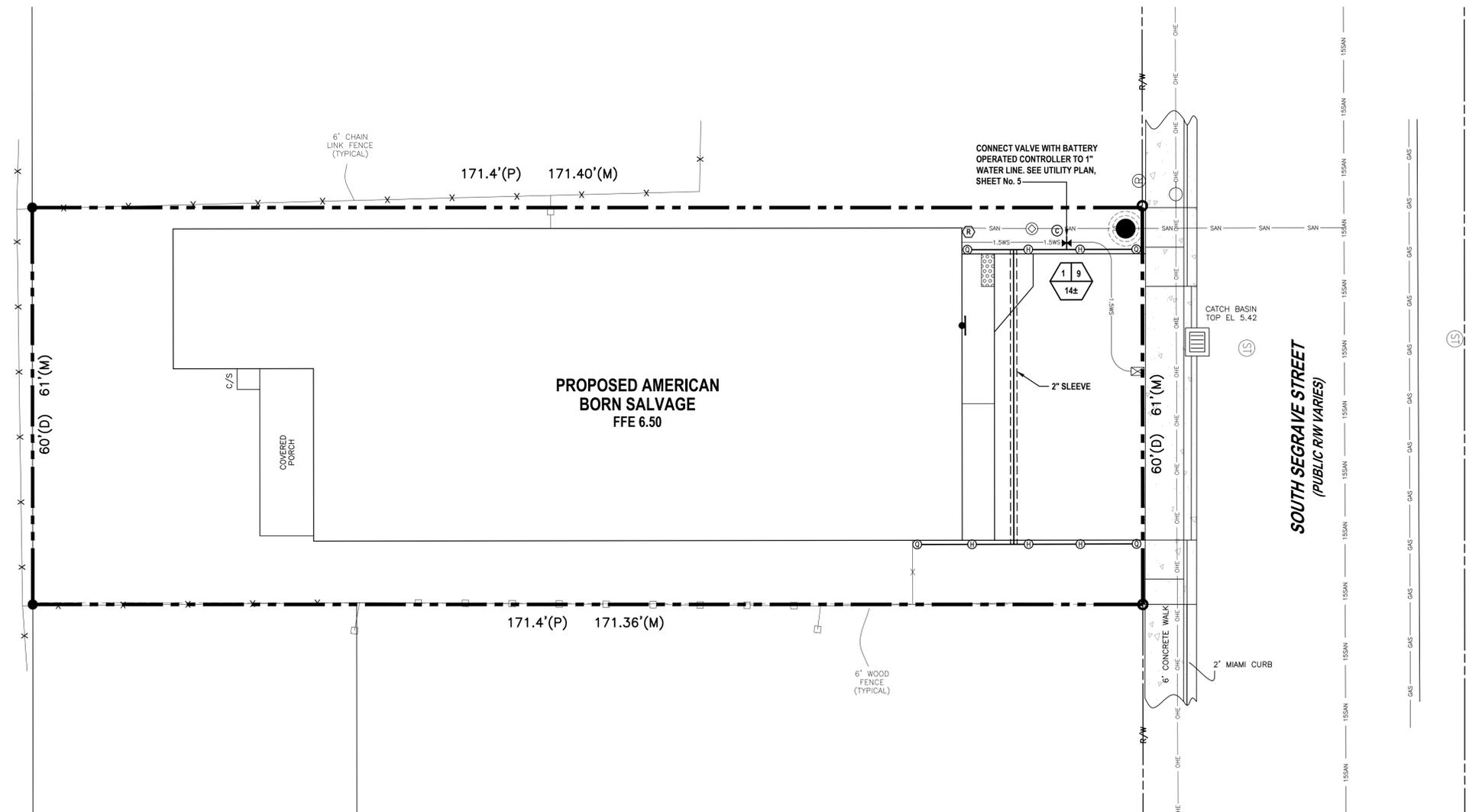
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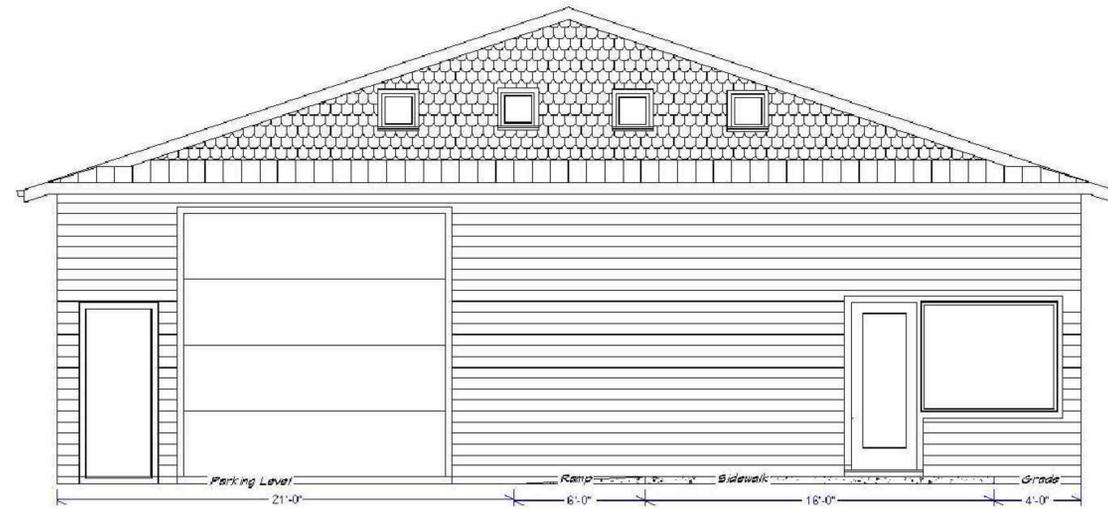
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FRONT ELEVATION

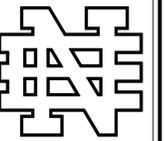
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PROPOSED ELEVATION RENDERING



EXISTING ELEVATION



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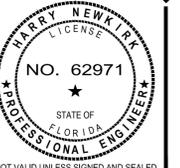
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BUILDING ELEVATIONS
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